

North Lake Specific Plan

RECOMMENDED

November 2025



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Table of Contents

1. Introduction.....1

- 1.1 Specific Plan Area.....4
- 1.2 Purpose5
- 1.3 Relationship to Other Documents5
- 1.4 Planning Process & Outreach9
 - Community Priorities.....14
- 1.5 Specific Plan Organization.....16

2. Background.....17

- 2.1 Community & Historic Context.....19
- 2.2 Existing Land Uses, Urban Form,
& Public Realm.....25

3. Vision, Goals & Policies 33

- 3.1 Vision36
- 3.2 Goals & Policies.....38

4. Zoning & Land Use 59

- 4.1 Applicability61
- 4.2 Zoning Districts62
- 4.2 Allowable Land Uses.....64

5. Public Realm Standards... 69

- 5.1 Sidewalks.....75
- 5.2 Parkways & Street Trees.....80

6. Private Realm Standards . 85

- 6.1 Scale90
- 6.2 Frontage.....100
- 6.3 Open Space.....109
- 6.4 Parking114

7. Implementation & Administration 117

- 7.1 Implementation Actions120
- 7.2 Citywide Implementation Overview127
- 7.3 Funding136
- 7.4 Infrastructure141
- 7.5 Administration144

A. Appendices145

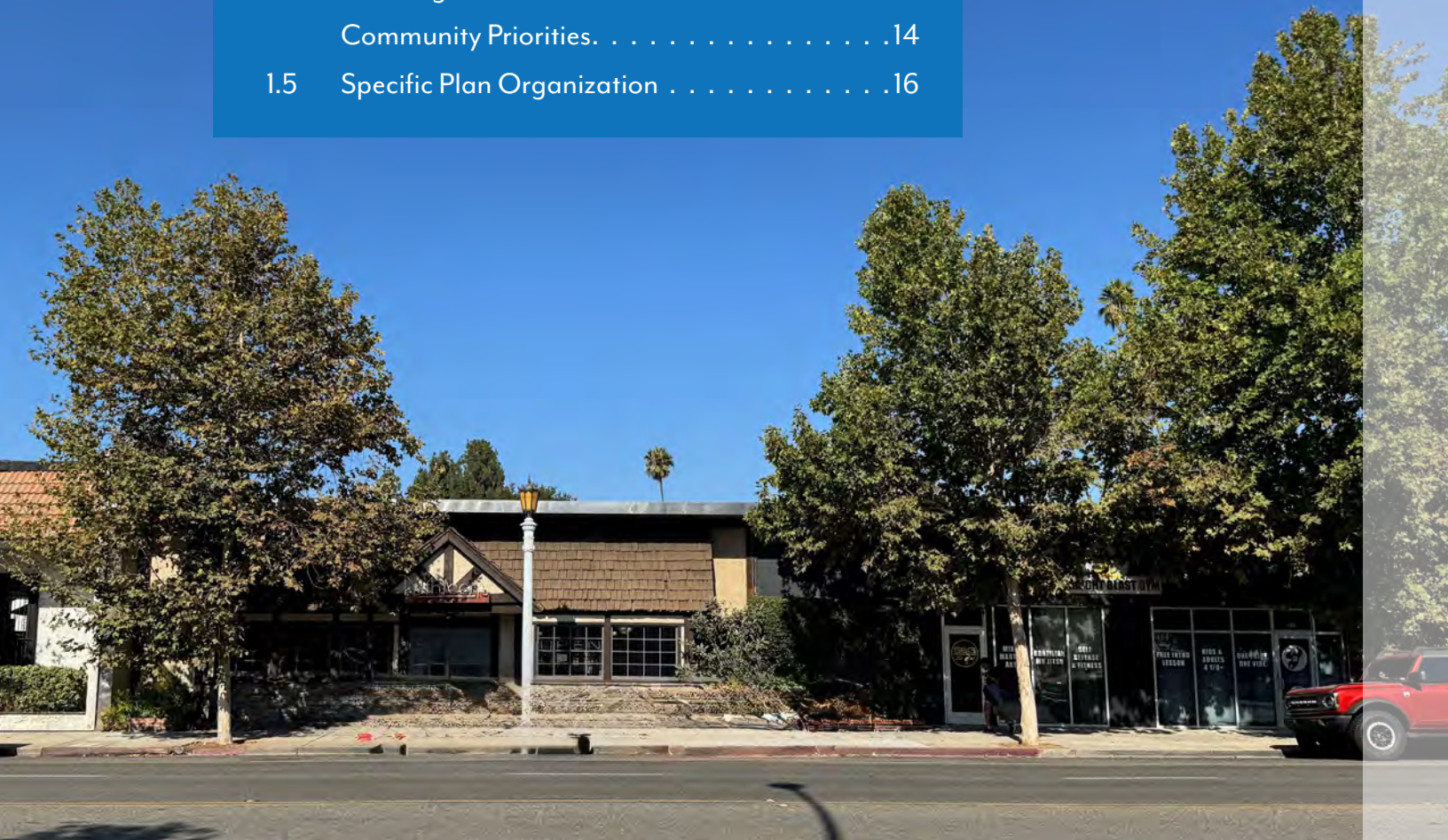
- A.1 Definitions146
- A.2 Design Guidance for Tree Selection148
- A.3 Subcommittee Recommendations168

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Ch. 1

Introduction

Introduction	3
1.1 Specific Plan Area	4
1.2 Purpose	5
1.3 Relationship to Other Documents	5
1.4 Planning Process & Outreach	9
Community Priorities.	14
1.5 Specific Plan Organization	16





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INTRODUCTION

The North Lake Specific Plan (NLSP) area serves as a primary northern gateway to the City of Pasadena. The Plan area centers along a prominent north/south corridor connecting with the Central District, the I-210 Freeway and the Metro A Line Lake Station to the south; the historic residential neighborhoods of Bungalow Heaven, Washington Square, and Historic Highlands to the east and west; and Altadena to the north. The Plan area also includes east/west portions of Orange Grove Boulevard, Washington Boulevard, and Villa Street, branching off of North Lake Avenue. These intersections act as important commercial nodes and crosstown connections.

Today, the NLSP is predominantly a commercial district whose character reflects the evolution of design and development trends over the past century of growth. However, with the adoption of the 2015 General Plan, there is an opportunity to incorporate medium-scale housing development on infill sites throughout the area, while preserving and celebrating historically significant properties and architectural characteristics and supporting a more pedestrian-oriented streets environment.

This Specific Plan establishes the following vision statement for the NLSP area, which reflects ideas and feedback from a multi-phase community engagement effort between 2018 and 2021:

“North Lake will be a vibrant and visually cohesive corridor, weaving together several distinct, pedestrian-oriented districts that complement and build upon the cultural and architectural history of the community and surrounding neighborhoods.”

The NLSP presents an opportunity for this vision to be grounded in policy and practice, and for the City and community to work together in confronting planning challenges and envisioning the future of the North Lake area. In addition to an extensive public outreach and visioning process, the NLSP represents the outcome of a multi-faceted outreach process and technical planning and design effort, directly informed by the perspectives and expertise of community members, City staff, the Planning Commission, Design Commission, Northwest Commission, and the City Council.

WHAT IS A SPECIFIC PLAN?

In the State of California, a Specific Plan is a regulatory tool that local governments use to implement their General Plan and to guide development in a localized area. The General Plan is the primary guide for growth and development in a community, and a Specific Plan focuses on the unique characteristics of a special area by customizing the land use regulations and development standards for that area. A Specific Plan establishes a link between the policies and implementation programs in the General Plan and individual development proposals in a defined area within the City.

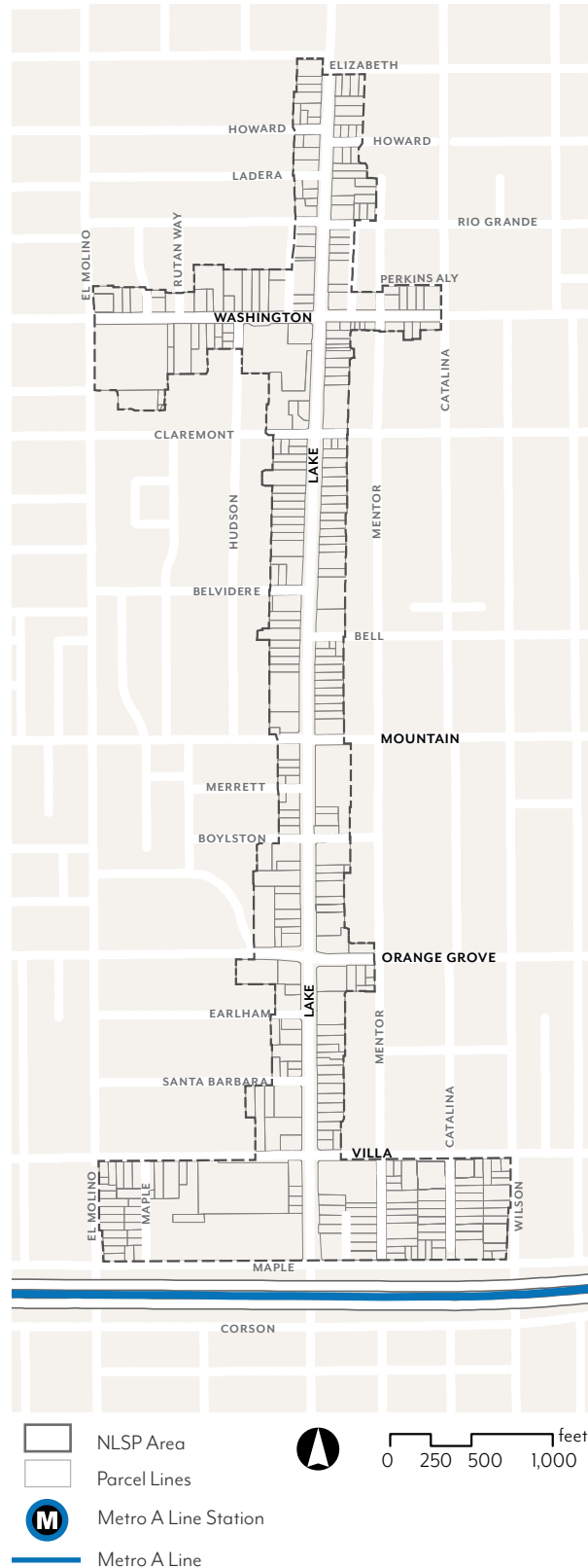
CHAPTER OVERVIEW

This chapter is organized into the following sections:

- » **1.1 Specific Plan Area**
- » **1.2 Purpose**
- » **1.3 Relationship to Other Planning Documents**
- » **1.4 Planning Process and Outreach**
- » **1.5 Specific Plan Organization**

1.1 Specific Plan Area

Map 1.1-1: North Lake Specific Plan Area



The NLSP area generally encompasses the area between Elizabeth Street to the north, Wilson Avenue to the east, Maple Street to the south and El Molino Avenue to the west. While the NLSP primarily regulates development along the Lake Avenue corridor, the standards and policies are influenced by, and intended to provide benefit to the surrounding residential communities, including landmark and historic districts of Bungalow Heaven, Washington Square, and Historic Highlands.



The NLSP area has a variety of architectural styles, many with traditional elements like arched openings.



Landsaped medians throughout the Plan area provide additional greening, slow down vehicles, and create a sense of place for pedestrians.

1.2 Purpose

The NLSP sets out a plan to facilitate investment, development, and improvements to the Plan Area, that will ultimately help realize the community's vision for North Lake Avenue. The NLSP optimizes land uses to increase opportunities for financially feasible commercial and residential developments, and help ensure that new development, sidewalk improvements, and added amenities contribute positively to the pedestrian experience. The NLSP includes standards to protect historic resources and support existing elements of the area that provide a sense of place and distinct character, while providing contextual standards for new development.

Future redevelopment opportunities will focus on infill development and making incremental changes to the area's built environment and land use mix. This document will be used by property and business owners, grant-seeking nonprofits, developers, decision makers, and City staff as the regulations that will guide private and public development projects. While the specific plan introduces a framework and toolkit for designing and implement future developments, it does not mandate or accelerate any specific projects or immediate changes to the built environment.

1.3 Relationship to Other Documents

General Plan

The NLSP is one of eight Specific Plans that serve to implement the City's 2015 General Plan Land Use and Mobility Elements. The General Plan contains eight Guiding Principles and a series of goals and policies that demonstrate the relationship between land use and high-quality design, the arts and culture, sustainable infrastructure, a vital economy, exemplary public services, and public involvement and participation.

The NLSP mirrors and builds upon the General Plan's policies, consistent with the General Plan's vision and guidance. It seeks to focus development around transit, and create a pedestrian-oriented gathering place for surrounding neighborhoods near the Lake/Washington intersection, and create a well-designed and vibrant corridor with a mix of uses that support multiple modes of travel. Projects that are consistent with the NLSP policies and development standards will in turn be consistent with the General Plan Guiding Principles, goals, and policies.

While the NLSP establishes an overall vision for North Lake Ave, the policies and standards herein apply specifically to private development and adjacent sidewalks. The NLSP serves as one of many complementary tools the City uses to implement the General Plan, and to meet sustainability goals through prioritization and guidance for private and public investments.

SUMMARY OF 2015 GENERAL PLAN GUIDING PRINCIPLES

1. Growth will be targeted to serve community needs and enhance the quality of life.
2. Pasadena's historic resources will be preserved.
3. Pasadena will be an economically vital city by providing jobs, services, revenues, and opportunities.
4. Pasadena will be a socially, economically, and environmentally sustainable community.
5. Pasadena will be a city where people can circulate without cars.
6. Pasadena will be a cultural, scientific, corporate, entertainment and education center for the region.
7. Community participation will be a permanent part of achieving a greater city.
8. Pasadena is committed to public education and a diverse educational system responsive to the broad needs of the community.

General Plan Land Use Element, 2015

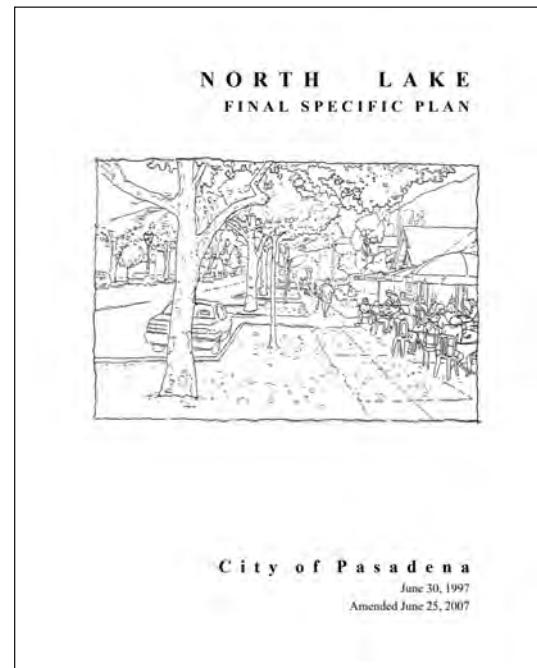
GENERAL PLAN GOAL 36: NORTH LAKE

“A well-designed and attractive corridor supporting multiple travel modes including transit, bicycling, and walking with clusters of distinctive places for shopping, dining, and living.”

GENERAL PLAN POLICIES

- » **36.1 Economic Vitality of the Avenue.** Transform North Lake Avenue from a low-density strip commercial corridor to series of distinctive low- to medium-density, pedestrian oriented, mixed-use villages designed and scaled to transition with adjoining residential neighborhoods where residents and visitors can live, shop, work and be entertained.
- » **36.2 Lake Transit Village.** Provide for the development of higher intensity mix of retail, office, and multi-family housing uses in proximity to the Metro Gold Line station that expanded the customer base for local businesses and support Metro Gold Line ridership contributing to the reduction of vehicle trips, energy consumption, and GHG emissions.
- » **36.3 Lake-Washington Neighborhood Village.** Promote the development of additional commercial uses with housing and pedestrian-oriented plazas and amenities to establish a “village-like” environment where people shop, dine, meet friends, linger, and actively use public places.
- » **36.4 Neighborhood Compatibility.** Require that the types of use and location, scale, and design of development buffer commercial and mixed-use development on Lake Avenue from adjoining lower residential neighborhoods.
- » **36.5 Transitional Heights and Setbacks.** Protect adjacent Low Density Residential and Low-Medium Density Residential areas north of Orange Grove Boulevard, which contain a number of locally and nationally recognized historic properties and districts, from the development of mixed use or residential projects by requiring appropriate transitional heights. Mixed use or residential projects proposed in these locations shall be limited to no more than three stories in height abutting the Low and Low-Medium residential areas. Appropriate setbacks shall also be established in order to provide further protection.
- » **36.6 Accessibility.** Design Lake Avenue and Washington Boulevard as complete streets that accommodate transit, bicycle, and pedestrian use. Include wider sidewalks, public plazas, bike lanes, and bicycle parking.

General Plan Land Use Element, 2015



Previous North Lake Specific Plan

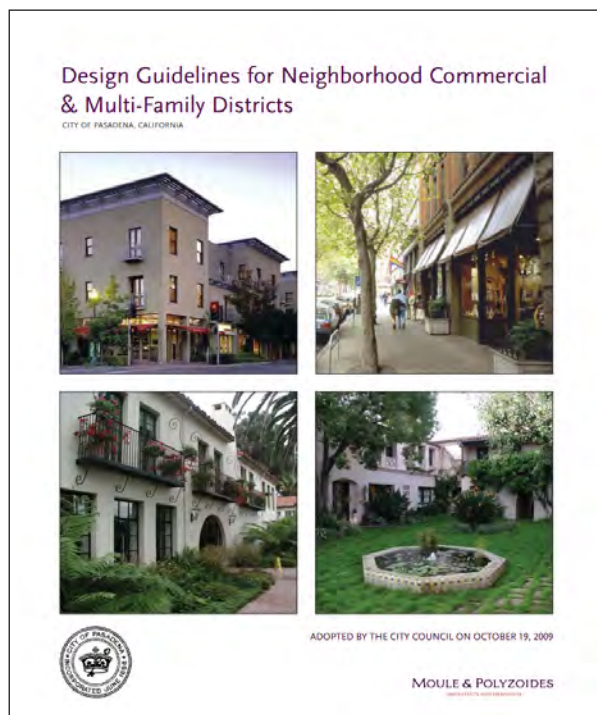
The NLSP builds on many of the objectives from the original 1997 NLSP (amended in 2007) with updated goals, policies, land use regulations, and development standards that address current and future current needs, opportunities, and challenges. The previous Plan's aspirations for Lake Avenue to grow beyond its role as a major north/south mobility corridor and meet its full potential as a thriving center of activity for residents of surrounding neighborhoods remain integral to the goals, policies, and standards of the NLSP update. This update of the NLSP further supports the goals and policies set forth in the General Plan.

Zoning Code

The Pasadena Municipal Code (PMC) is one of the primary tools for implementing the City's General Plan. Specifically, Title 17 of the PMC, or the Zoning Code, describes allowable land uses, development standards, and permit requirements for each zoning district in the City. The zoning districts and associated land use regulations and development standards established by the NLSP will be codified in the Zoning Code.

Design Guidelines for Neighborhood Commercial & Multi-Family Districts

The Design Guidelines for Neighborhood Commercial & Multi-Family Districts (October 2009) supplement the design-related goals and policies of the General Plan Land Use Element and offer more direction for



proceeding with the design of a project. The guidelines illustrate options, solutions, and techniques to achieve the goal of excellence in new design specifically for commercial, residential, and mixed-use buildings that are subject to design review. The guidelines are not zoning regulations or development standards, but rather performance goals that apply to areas within the City that do not have detailed guidelines or supplement existing guidelines, including those in the NLSP.

Sign Design Guidelines

The Sign Design Guidelines provide guidance in the way signs are designed, constructed, and placed in order to further implement the purposes of Chapter 17.72 (Sign Regulations) of PMC. The guidelines are intended to provide good examples of techniques that should be used in order to meet the City's expectations for quality business signage to be applied during the City's design review process or Master Sign Plan process.

Bicycle Transportation Action Plan

The Department of Transportation's Bicycle Transportation Action Plan (BTAP) (August 2015) provides specific goals, objectives, actions, and timeless for creating an environment (1) where people circulate without a car, (2) that significantly increases the number of people who commute by bike, (3) that increases the number of people who commute by bike, (3) that increases the number of people who use a bike for utilitarian trips, fitness and recreation, and (4) that provides business and economic benefits for the City. The BTAP provides details for a network of bikeways so that

every neighborhood is within a quarter mile of an effective bicycle route and funding strategies to implement the plan. The NLSP area contains a portion of the BTAP's Villa Street buffered bike lane or cycle track route that stretches from Champlain Avenue to Hill Avenue. While outside of the plan area, a bicycle lane running east-west on Washington Boulevard from Forest Avenue to El Molino Avenue are located immediately west of the plan area boundaries. The NLSP also supports the BTAP through promoting enhanced sidewalks that allocate space for bicycle parking and supporting bicycle infrastructure within the plan area.

Active Transportation Plan

The Department of Transportation's Active Transportation Plan would integrate, complement, and update previous City planning documents, including the City's 2024 Pedestrian Transportation Action Plan, 2021 Greenways Feasibility Study, City's 2015 Bicycle Transportation Action Plan, and 2006 Pedestrian Plan to create a comprehensive action plan for safe and accessibility walking, biking, and rolling in the City. The community-driven plan will focus on four key areas: implementation strategy development, prioritization and planning for future active transportation infrastructure improvements, consistency with the Mobility Element of the City's General Plan and incorporating the latest best practices in active transportation.

Local Roadway Safety Plan

Local Roadway Safety Plan (LRSP) identifies roadway safety solutions by analyzing citywide collision data and prioritizing them based on cost-benefit and efficacy analysis. The LRSP was prepared in August 2022, consistent with California Department of Transportation (Caltrans) program guidelines. This program and subsequent analysis provide a uniform set of criteria for the state of California to evaluate and provide funding to local agencies for local roadway safety improvements.

Local Roadway Safety Action Plan

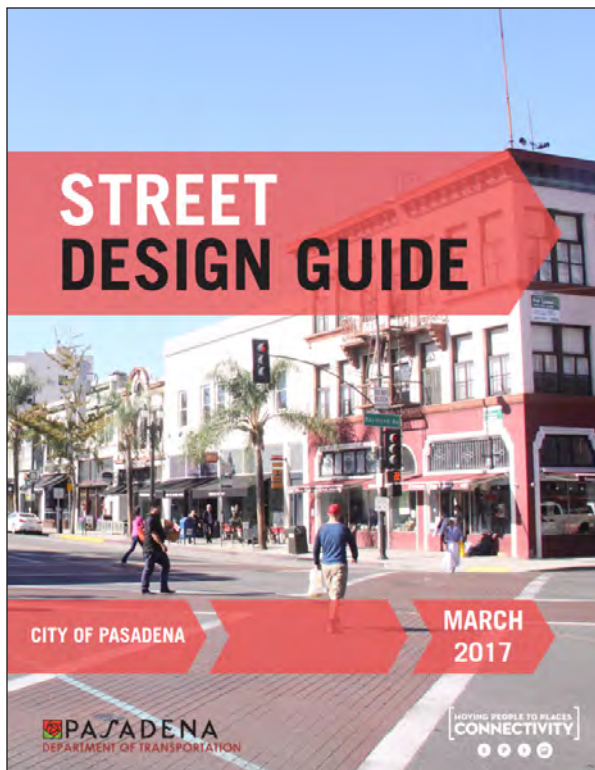
Local Roadway Safety Action Plan (LRSAP) is a traffic safety plan that incorporates inclusive public and stakeholder engagement into the detailed collision analysis of the 2022 Local Roadway Safety Plan as a prerequisite for a United States Department of Transportation (USDOT) federal Safe Streets and Roads for All (SS4A) Implementation Grant Program. The LRSAP identifies and prioritizes projects from various City traffic safety plans, such as the Bicycle Transportation Action Plan, Pedestrian Transportation Action Plan, Capital Improvement Program, and Local Roadway Safety Plan, on a uniform set of metrics for future USDOT SS4A Implementation Grant funding opportunities.

North Lake Avenue Traffic and Pedestrian Safety Enhancement Plan

Part of the Pasadena Transportation Action Plan (PTAP), the North Lake Avenue Traffic and Pedestrian Safety Enhancement Plan was programmed by the City of Pasadena to focus on enhancing traffic and pedestrian safety along North Lake Avenue Between Maple and Mountain Streets. The segment length of 0.6 mile-long is intended to identify concerns and opportunities to enhance traffic and walkability along the corridor. The North Lake Avenue & Pedestrian Safety Enhancement Plan project team has engaged in several efforts to understand the existing nuances of the corridor. Such efforts include examining and analyzing roadway characteristics, traffic volumes, and collisions. The Plan is consistent with the Pasadena General Plan goals of creating a pedestrian-friendly environment and enhancing the streetscape. As the project prioritizes access for people of differing abilities by providing more accessible pedestrian crossings and maintaining curb access for paratransit users, it is consistent with the goals of the Complete Streets Act of California.

Pasadena Street Design Guide

The Department of Transportation's Pasadena Street Design Guide (March 2017) implements the 2015 General Plan Mobility Element Complete Streets Policy, including the following goals and objective: (1) Streets should reflect neighborhood character and accommodate all users; (2) Complete Streets should accommodate all users such



as pedestrians, bicyclists, public transit, skateboarders and scooter; and (3) Streets should reflect individual neighborhood character and needs, and support healthy activities such as walking and bicycling. The NLSP references the Street Design Guide as it applies to sidewalks, parkways, and street trees, which fall under Chapter 5 (Public Realm) of the NLSP.

Greenways Feasibility Study

Building upon the efforts of the 2015 Mobility Element and associated Bicycle Transportation Action Plan, the City of Pasadena commissioned a detailed feasibility study to further develop improvements on four north-south bicycle boulevard corridors, or Greenways. The study evaluates traffic calming features based on existing traffic conditions, bicycle level of traffic stress, vehicle level of service, and cost of corridor improvements, and proposes planning level concept plans for each Greenway. Two of the Greenways identified are El Molino Avenue and Wilson Avenue, located a quarter mile to the west and east of Lake Avenue, respectively. On El Molino Avenue parallel to the NLSP area, the study proposes Class III bicycle boulevard using "sharrow" markings and offset edge islands to create traffic-calming chicanes. On Wilson Avenue parallel to the NLSP area, the study proposes a Class III bicycle boulevard using "sharrow" markings, mid-block traffic-calming islands and diverters, and painted bike intersection crossings at Orange Grove Boulevard. Both of these north-south greenways would connect to buffered bike lanes on the I-210 overcrossings, improving bicycle access to the Central District and the two-way cycle track along Union Street.

Master Street Tree Plan

The Department of Public Works' Master Street Tree Plan serves as the guiding document that designates the official tree species to be planted on a block-by-block basis throughout the City. The NLSP references the Master Street Tree Plan in Appendix A.2 (Design Guidance for Tree Selection) to guide discussions between the City and community when updating the Master Street Tree Plan for the area. Appendix A.2 includes a description of the existing street trees along North Lake Avenue within the plan area, followed by recommendations for potential new species.

Pasadena Climate Action Plan

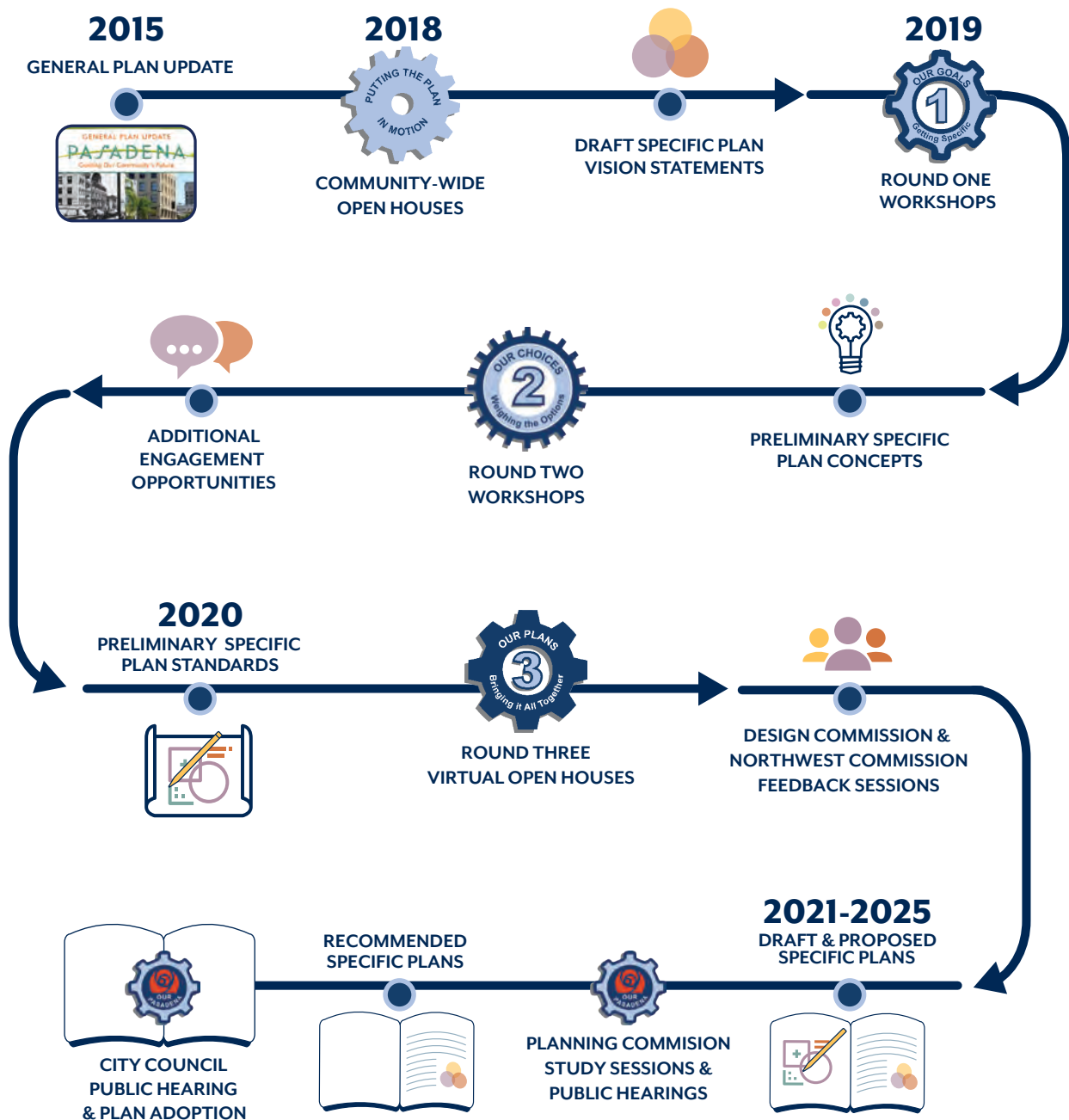
The Pasadena Climate Action Plan (CAP) (March 2018) provides a strategic framework measuring, planning, and reducing the City's share of greenhouse gas (GHG) emissions with the goal of reducing emissions by more than half by the year 2035. The NLSP supports the CAP and the identified strategies to reduce GHG through sustainable land use and pedestrian infrastructure as well as urban greening, which are addressed in Chapter 4 (Land Use), Chapter 5 (Public Realm), and Appendix A.2 (Design Guidance for Tree Selection).

1.4 Planning Process & Outreach

1.4.1 OUR PASADENA PROGRAM

The General Plan is a document that outlines the community's vision for Pasadena over the next 20 years. As an overall visioning document, the General Plan's goals and policies are implemented in various ways, including Specific Plans. *Our Pasadena – Putting the Plan in Motion* is the City's General Plan implementation program. Focused on updating Pasadena's Zoning Code and establishing neighborhood-specific design and land-use goals for the City's eight Specific Plans: Central District, East Colorado, East Pasadena, Fair Oaks/Orange Grove, Lamanda Park, Lincoln Avenue, North Lake, and South Fair Oaks.

The NLSP is informed by a thorough planning and public outreach process led by the City's Planning & Community Development Department, and supported by a consultant team of urban planners, urban designers, outreach specialists, economists, architects, and landscape architects. The planning process involved coordination with staff from various other City of Pasadena departments as well, including Public Works, Transportation, Economic Development, Northwest Programs, and the City's Accessibility Coordinator.



1.4.2 PUBLIC OUTREACH

Throughout the planning process, the City solicited input from residents, property owners, businesses, community leaders, and other stakeholder groups through a variety of outreach events, public meetings, interviews, and online engagement tools. The following list provides a summary of public outreach methods, workshops, meetings, and hearings, and an overview of key recurring feedback themes from the outreach process.

Community-Wide Open Houses

March 2018

In 2018, the City initiated the Our Pasadena Program to review and update the City's eight specific plan areas. The program's primary objective was to establish neighborhood-specific goals and policies resulting in a refined set of permitted uses, residential densities, and development standards and guidelines that will shape the built environment along the City's major commercial and mixed-use areas within the eight planning areas in accordance with the City's General Plan and Guiding Principles. The program kicked-off with a series of open houses to introduce the program and solicit feedback on the general vision on each of the eight specific plan areas, including North Lake Avenue.

Round 1 Workshop

June 21, 2018

In the Round 1 Workshop, the Planning & Community Development Department facilitated a listening and learning session to find out more about participants' experience living and/or working in, or visiting the NLSP area, and their ideas for how the NLSP could improve the area. The workshop began with a brief interactive visioning activity, and an introductory presentation on the NLSP Update program, the General Plan vision for the NLSP area, and background information. For the majority of the workshop, participants were divided into small groups with facilitated discussions on specific topics such as appropriate land uses and housing types, streetscape and public amenities, and mobility and parking. The main objective of the workshop was to solicit feedback from the community, rather than reach consensus on any particular topic.

Community Walking Tour

April 6, 2019

The Planning & Community Development Department hosted a one-mile walking tour of the NLSP area, starting at the corner of Washington Boulevard and El Molino Avenue and ended at the Santa Catalina Branch Library. Participants wrote comments on worksheets as they walked east along Washington Boulevard, then north up Lake



Round 1 Workshop



Community Walking Tour



Community Walking Tour

Avenue, and back down to Washington Boulevard. At the end of the tour, participants identified specific locations on large maps with image cards depicting desired uses. The following represents major themes that emerged during the walking tour: residential development, Washington Theater, Washington Boulevard/Lake Avenue intersection, active sidewalks and storefronts, pedestrian experience, automotive uses, and parking requirements.

Round 2 Workshop

May 30, 2019

The Planning & Community Development Department hosted a second community workshop to present preliminary land use and urban form concepts that considered a number of factors, including community feedback received since the first workshop. Input received helped to refine these concepts and guide the drafting of goals, policies, and development standards. In an opening icebreaker activity, participants were able to reaffirm what we heard so far from the community by placing stickers next to those comments. Next, City staff presented background information on the program, an overview of the existing NLSP area, emerging themes and draft vision. Lastly, participants formed small groups to discuss the concepts. Each table reported back to the entire group with a summary of the main points.

Youth Summit

October 19, 2019

The Planning & Community Development Department hosted an Our Pasadena Youth Summit for students at the Robinson Recreation Center. Through the use of multimedia tools and interactive activities, including a virtual reality tour through parts of the City, the event introduced city planning to youth and allowed participants to share their unique perspective on what they think will make Pasadena a better place now and in the future.

Round 3 Virtual Open House

October 2020 - March 2022

Live Webinar: October 15, 2020

For the third and final round of community workshops, the Planning & Community Development Department hosted an interactive virtual open house website and live webinar. Through an introductory presentation and a series of informational materials, staff presented the refined NLSP vision and concept, along with full draft standards for the Land Use, Public Realm, and Development & Design chapters of the plan. Participants were encouraged to submit questions in the Q&A portion of the live webinar event. While the community was unable to gather in person due to the COVID-19 pandemic, the virtual platform was available 24/7 for an extended period of time, allowing participants to visit and provide feedback at their pace and convenience, as well as download materials and share the open house site with family, friends, and neighbors.



Round 2 Workshop



Youth Summit



Round 3 Workshop - Virtual Open House

Northwest Commission Meeting

April 13, 2021

Following the Round 3 Webinar, the Planning & Community Development Department presented at the Northwest Commission in Spring of 2021 to solicit feedback on the preliminary draft of the NLSP, respond to clarifying questions from commissioners, and discuss various issues to be considered in the development of standards, policies, and implementation strategies for the next draft of the plan.

WHAT WE HEARD

Participants shared a wide range of input throughout the outreach process, which included the following recurring themes:

- » Enhance the pedestrian experience through the usage of well-maintained street trees, lighting, benches, and other streetscape amenities.
- » Reinforce the North Lake Avenue and Washington Boulevard intersection as a focal point of community activity offering a variety of commercial uses, housing options, and public gathering spaces to foster a stronger sense of place and more pleasant walking environment.
- » Enhance North Lake Avenue's streetscape to support safe pedestrian and bicycling, and use of public transportation.
- » Create a safe environment for using public transportation, specifically fostering a welcoming public realm surrounding the Metro station.
- » Encourage the adaptive reuse of existing residential buildings between Claremont and Mountain Street for compatible uses, while allowing for a mix of residential and commercial uses.
- » Recognize the importance of the area's historic and cultural resources and utilize them as the basis for strengthening the district identity for North Lake.
- » Encourage the development of more housing options that support affordable housing.
- » Provide opportunities for the development of higher intensity commercial and residential uses with public gathering spaces south of Orange Grove Boulevard with increased access to the Metro Lake Station and improved walkability.
- » Continue to provide opportunities for new housing that complements existing uses and adjacent neighborhoods, with an emphasis on housing that is affordable for people who work in the community.
- » Enhance the aesthetics and design of new development through strengthened design and development standards, including provisions for building form transitions sensitive to adjacent single-family residential neighborhoods and historic landmark districts.
- » Encourage new development that is consistent with the existing character and design in this area.
- » Incorporate active ground-floor uses and pedestrian-oriented design treatments along North Lake Avenue and intersecting streets that support a more vibrant street atmosphere, increase greenery and better connect the corridor with surrounding neighborhoods.
- » Encourage accessibility in design of the public realm and new developments.



Community members sharing their feedback at an in-person public workshop for residents and other stakeholders.

Design Commission Meeting

May 18, 2021

Community Development Department staff presented the preliminary draft of the NLSP to Design Commission in late spring of 2021, focusing on new public realm and development standards that would impact the scale and design of new construction in the North Lake area. Commissioners asked thoughtful questions, discussed considerations from their own expertise, and provided specific input on how the plan and its regulations could be improved in subsequent iterations.

Planning Commission Meetings

Planning Commission Study Sessions:

May 24, 2023; January 10, 2024; April 10, 2024; May 8, 2024; November 13, 2024; July 23, 2025

The Planning Commission held six Study Sessions to discuss the NLSP between May 2023 and November 2024. At the conclusion of the May 8, 2024 Study Session, the Planning Commission voted to create a North Lake Avenue Subcommittee to draft recommendations for public realm improvements. The April 10, 2024 Study Session, focused on drive-through businesses and potential redistribution of residential density. On July 23, 2025, the Study Session focused on how the Subcommittee's recommendations were incorporated in the draft NLSP.

North Lake Avenue Subcommittee:

May 28, 2024; June 7, 2024; July 15, 2024; August 17, 2024; August 26, 2024; September 9, 2024; November 13, 2024

As part of their effort, the Subcommittee held several meetings with City staff and members of the community. On May 28, 2024, the Subcommittee met with City Staff (Planning and Public Works) to review background information on the public realm, and on June 7, 2024, a member of the Subcommittee met with Department of Transportation staff to review the current public realm plans for North Lake Avenue. The Subcommittee also met with the Executive Director of the South Lake Business Association on July 15, 2024 to learn about the processes to form and manage a Business Improvement District. In addition, the Subcommittee held a meeting with invited members of the community on August 17, 2024, with over 20 total attendees. At that meeting, a 10-member working group

was formed, which met on August 26, 2024 and September 9, 2024.

Based on these meetings and discussions, the Planning Commission Subcommittee (comprised of Commissioners Cole, Delgado, Hunt Hernandez, and Sims) provided detailed input related to the public realm in the NLSP area, including goals, objectives, policies, and implementation actions with timelines. The Commissioners elaborated on the draft NLSP Vision statement, offering a vision for a comprehensive streetscape plan that would transform North Lake Avenue into a Great Street.

Their vision emphasizes the importance of a cohesive, active, welcoming, and thoughtfully designed public realm in creating a memorable destination and inclusive public arena for commerce and gathering. Acknowledging that North Lake Avenue currently lacks a cohesive identity in its streetscape and surrounding building forms, their vision uses the metaphor of an 'outdoor living room' to describe how both built and natural elements of the streetscape create a secure, well-defined space that invites people to meet, connect, linger, and conduct business comfortably:

"Appropriate building frontages will form the 'interior walls' of the outdoor room; the sidewalks, landscaping, roadways, and related public open spaces and greens its 'floor'; its 'ceiling', the canopy of trees, overhangs, awnings, or umbrellas that modulate our experience of the weather and sky above. Experiencing North Lake Avenue will be protective while celebrating its history and mountain views."

The Subcommittee's goals are summarized on the next page as Community Priorities for Transformative Placemaking and Streetscape Design on North Lake Avenue, and their work has influenced the goals, policies, standards, and implementation actions of the NLSP. The Subcommittee's recommendation letter is also included as Appendix A.3.

Planning Commission Hearing:

September 10, 2025

Following the Study Sessions, staff made changes to the draft NLSP incorporating the comments of the Commission and its Subcommittee throughout. Staff brought the Plan back for a hearing in early Fall 2025, where the public was invited to share feedback, the Commission discussed the proposed Plan, and voted to recommend approval to the City Council.

COMMUNITY PRIORITIES

for Transformative Placemaking and Streetscape Design on North Lake Avenue

- » **Sense of Place:** Create a Great Street with a strong sense of place through public realm design that offers safe and visually interesting pedestrian connections, generates community gathering and activity, and respects, preserves, and celebrates North Lake Avenue's historic fabric and context.
- » **Mixed-Use District:** Foster a supportive environment for investment in businesses and development of mixed-use projects that help meet housing and retail service needs within the NLSP area and its surrounding neighborhoods.
- » **Well Maintained Corridor:** Provide a high standard of care and maintenance that contributes to a clean and safe environment along the entirety of the corridor and uplifts North Lake's feeling of community pride and ownership.
- » **Traffic Calming:** Support strategies to calm traffic movement along North Lake Avenue such as public realm infrastructure and streetscape design that promote pedestrian travel and bicycle use and safety.
- » **Managed Congestion:** Encourage a metered pace of travel along the corridor that reflects North Lake Avenue's identity as a pedestrian-scaled social and economic destination via all modes of transportation, not simply a vehicle thoroughfare.
- » **Accessible Destination:** Reinforce North Lake's identity as a destination by improving public and active transportation connections to and within the NLSP area rather than prioritizing movement through the area to other destinations.
- » **Sustainable District:** Advance climate change mitigation goals, reduce environmental impacts, and support a walkable and sustainable environment through strategies that de-emphasize single-occupancy vehicles, reduce impermeable paving, increase tree canopy, and create memorable and delightful experiences for pedestrians.
- » **Green and Creative Landscaping:** Promote creative and sustainable landscaping techniques that beautify the corridor while mitigating the environmental impacts of stormwater runoff.
- » **Public Art and Community Identity:** Encourage murals and public art programming that reinforce a strong sense of place and continuity between North Lake's past, present, and future.



A workshop hosted by the Planning Commission Subcommittee.

City Council Hearing

Date to be determined

Section to be completed after Council hearing.

Mailings & Promotional Materials

In addition to public outreach events and workshops, the Planning & Community Development Department has advertised the NLSP and provided program updates through the following platforms and publications:

- » Mailers to property owners, occupants, and renters within the NLSP area and within 500 feet of the NLSP area boundaries
- » E-mailing list for program newsletter subscribers
- » OurPasadena Program website
- » OurPasadena and Citywide social media accounts
- » Council District Newsletters
- » City of Pasadena InFocus
- » Local press coverage

1.4.3 ENVIRONMENTAL CLEARANCE

In the 2015 Pasadena General Plan update, the City prepared a programmatic General Plan Environmental Impact Report (GP EIR) to analyze potential citywide impacts, broad policy alternatives, and programmatic mitigation measures associated with the update of the General Plan and specific plan amendments. An Addendum to the GP EIR was prepared to address potential site-specific environmental impacts associated with the update to the NLSP.

Per the GP EIR, future discretionary review may rely on the analysis provided in the GP EIR for the purpose of tiering and/or streamlining. The purpose of tiering is to use the analysis of general matters contained in a broader EIR (such as the GP EIR) with later California Environmental Quality Act (CEQA) documents on narrower or site-specific projects. Tiering serves to reduce repetitive analysis and provide subsequent site-specific analysis at a time when it is meaningful.

1.5 Specific Plan Organization

The NLSP is organized into seven chapters and multiple appendices, as described below.

Ch. 1 - Introduction

This chapter presents the purpose of the NLSP, discusses the relationship of the NLSP to other planning documents, outlines the planning outreach process, and summarizes community feedback and the Planning Commission's priorities for North Lake.

Ch. 2 - Background

This chapter provides additional historical context for the NLSP area and identifies challenges and opportunities within the plan area's existing conditions.

Ch. 3 - Vision, Goals & Policies

This chapter establishes the overall vision for the NLSP area, and specific visions for the subareas. The vision is followed by goals and policies by subarea and topic.

Ch. 4 - Zoning & Land Use

This chapter introduces the zoning districts for the NLSP and establishes the types of land uses allowed for potential new development within each zoning district.

Ch. 5 - Public Realm Standards

This chapter presents standards and guidelines for the public realm adjacent to new development, including sidewalks, parkways, and street trees.

Ch. 6 - Private Realm Standards

This chapters presents standards and guidelines for development of private property, including allowable densities and heights, as well as required setbacks, open space, and parking standards.

Ch. 7 - Implementation & Administration

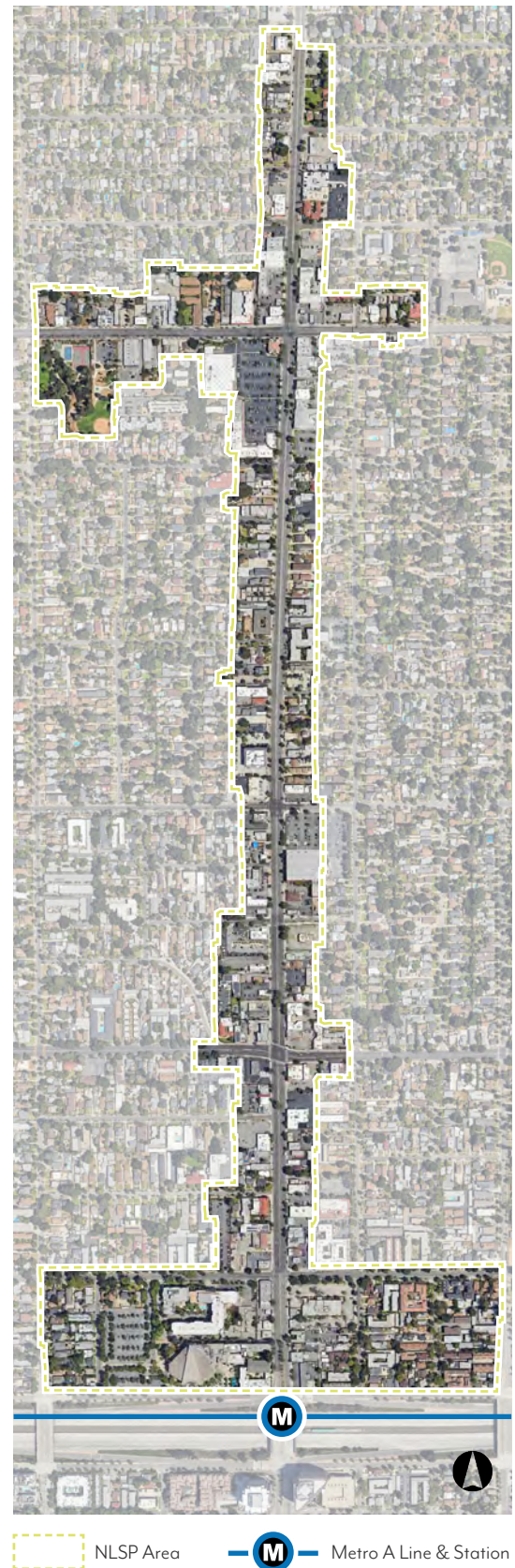
This chapter presents implementation actions and responsibilities, and potential programming and funding opportunities to bring the NLSP vision to life.

Appendices

A.1 – Definitions provides a glossary of land use planning and urban design terminology used throughout the document.

A.2 – Design Guidance for Tree Selection supplements the public realm standards introduced in Chapter 5 with a detailed overview of existing street tree conditions, and recommendations for tree species to be incorporated in future updates of the City's Master Street Tree Plan.

A.3 – Subcommittee Recommendations includes the full text of a letter written by the Planning Commission Subcommittee.



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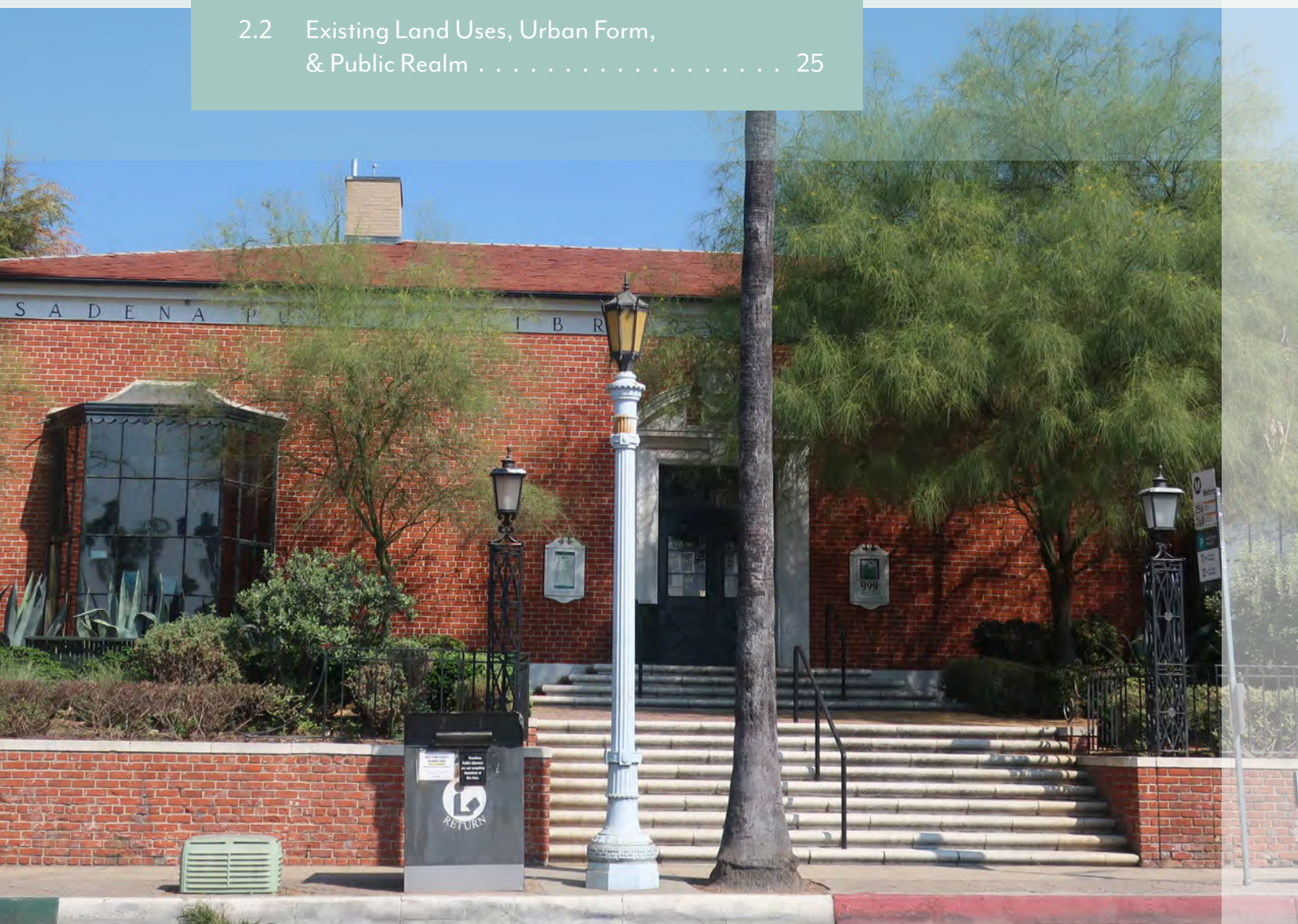


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Ch. 2

Background

- 2.1 Community & Historic Context19
- 2.2 Existing Land Uses, Urban Form,
& Public Realm 25



2.1 Community & Historic Context

From its origin as a burro trail in the 1860s, Lake Avenue today serves as a primary northern gateway to the City, and a central north/south thoroughfare connecting Pasadena's historic residential neighborhoods of Historic Highlands, Washington Square, and Bungalow Heaven to the Metro A Line Lake Station, the I-210 Freeway, and the Central District to the south.

North Lake Avenue has long served as an important north/south route for regional connectivity. Formally opened as Lake Vineyard Avenue in 1886 to connect Benjamin Wilson's Lake Vineyard Ranch with the mountains, a horse-car line was established along the Avenue in 1888, followed by development of the Pacific Electric North Lake Avenue Streetcar Line in 1894. As a result of Pasadena's 1925 Bennett Plan, Lake Vineyard Avenue was widened in 1929 from a residential street to a major arterial on par with Colorado Boulevard to accommodate the automobile. In 1941, the North Lake Avenue Streetcar Line was abandoned and replaced with bus service.

Residential development from the early 20th Century began north of the NLSP area, gradually extending south to Villa Street. Commercial development soon followed, and by the 1920s, the Lake/Washington intersection was an

important retail center serving surrounding neighborhoods. Once containing large tracts that were filled with citrus and apricot groves and vineyards, properties along Lake Avenue were subdivided in the early 20th Century to accommodate housing for the growing population. Early homes were built in typical American styles of the period, with many utilizing 'arroyo stone' sourced from the nearby Arroyo Seco river.

In the 1920s, zoning districts were established citywide. The original zoning districts allowed for the development of residential bungalow courts, four-family flats, and single-family housing along North Lake Avenue, while commercial development was limited to the intersections at Maple Street, Orange Grove Boulevard, and Washington Boulevard. The Washington Theater building, which housed a theater, apartments, offices, and shops, was among Southern California's first mixed-use projects when constructed in 1924.

Economic growth and increased vehicle access supported the expansion of commercial uses from Orange Grove Boulevard to Maple Street in the southern portion of the NLSP area, and between Claremont Street and Elizabeth Street at the northern portion of the NLSP area. As Lake Avenue transitioned into an automobile thoroughfare,



The Washington Theater, built in 1924, in 1940. Image courtesy of the Archives at Pasadena Museum of History (T-2-12)

many of the pedestrian-oriented storefronts along Lake Avenue were replaced with vehicle-oriented uses, structures, and site layouts.

By the 1960s, the residential area between Claremont Street and Orange Grove Boulevard was zoned for neighborhood commercial uses. In 1978, a Zody's Discount Retailer (now CVS) opened, followed in 1986 by the 6-acre North Lake Square redevelopment project, which was anchored by a Boys Market (now Food4Less).

In 1997, the City Council approved the North Lake Specific Plan, which articulated a vision of a thriving integrated area of commercial, residential, cultural, recreational, and civic activities, featuring a diverse array of shops, services, residential developments, and neighborhood gathering spaces. The Specific Plan implemented the 1994 General Plan vision for the area and placed focus on revitalizing North Lake Avenue, building neighborhood identity and creating community cohesion. Areas within the corridor were identified for mixed-use development and aimed to promote a pedestrian friendly environment. Notably,

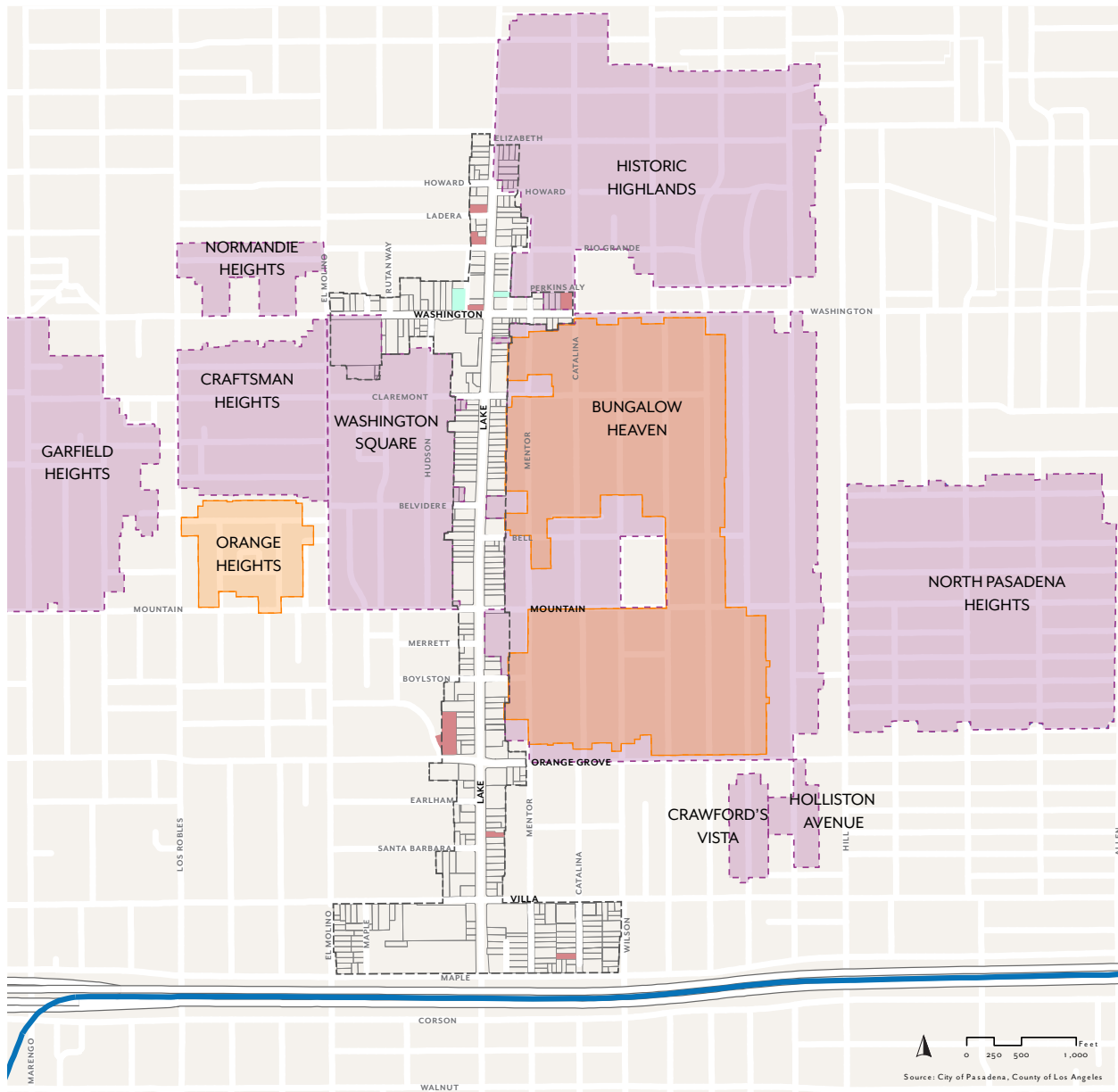
the 1997 NLSP shifted away from auto-oriented uses by prohibiting new drive-through services and new eating establishments with take-out service. The NLSP was amended in 2007 to revise the zoning district boundaries and designations for several parcels within the NLSP, however amendments did not introduce any changes to the overall plan vision or policies.

The 2015 General Plan Update maintains a primarily mixed-use environment throughout the NLSP area, with the exception of a commercial-only designation along Lake Avenue between Orange Grove Boulevard and parcels just north of Mountain Street, and an institutional designation on the Lake Avenue Church site on the northwest corner of Lake Avenue and Maple Street. Initiated in 2020, the Pasadena North Lake Avenue Traffic & Pedestrian Safety Enhancement Plan includes infrastructure improvements for pedestrians and bus riders in portions of the NLSP area along Lake Avenue between Mountain Street and Maple Street. Implementation of the Safety Enhancement Plan is in progress at the time of the NLSP update.



The Charles Francis Saunders house, home of the nature writer and amateur photographer, at 580 N. Lake Avenue was built in 1906 and is now a designated landmark. Image courtesy of the Saunders Collection at Huntington Library

Map 2.1-1: North Lake Specific Plan Designated Historic Resources



The NLSP area lies north of the Central District specific plan area. The Metro A Line runs through the southern portion of the SP area, and the 210 freeway meets the NLSP area at its southern terminus. The NLSP area is surrounded by a number of landmark districts including Historic Highlands, Washington Square, and Bungalow Heaven, and includes landmarked buildings throughout.

- North Lake Specific Plan Boundary
- Landmark District
- National Register Historic District
- Landmark Buildings
- Historic Signage
- M Metro Station
- Metro A Line

* Other historic resources, such as eligible resources are not mapped

Table 2.1-1: North Lake Specific Plan Area Historic Resources

Locally Designated Landmark Buildings		Individual Properties Determined Eligible in 1997 North Lake Specific Plan Survey	
Saunders House	580 N. Lake Avenue	1109 N. Lake Avenue	
Reynolds & Eberle Mortuary*	825 E. Orange Grove Boulevard	1540 N. Lake Avenue	
C.C. Loury House	397 N. Catalina Avenue	1550 N. Lake Avenue	
Oversen Building	1347 N. Lake Avenue	Landmark Districts Determined Eligible in 1997 North Lake Specific Plan Survey	
Santa Catalina Library	999 E. Washington Boulevard	960-1010 N. Lake Avenue (even side only)	
Commercial Building	1445 N. Lake Avenue	NOTE At the time of publication, a citywide historic resources survey that will re-evaluate undesignated properties, including those in the Specific Plan area, is underway. Those findings will be published separately. KEY * Appears eligible for listing in the National Register † Determined eligible for listing in the National Register	
Breiner House†	1495 N. Lake Avenue		
Washington Theatre*	851 E. Washington Boulevard		
Former Drive-In Market	731 E. Washington Boulevard		
Historic Signs			
Washington Theatre	851 E. Washington Boulevard		
Crown Hotel and Restaurant Supply Co.	1368 N. Lake Avenue		
Historic and Landmark Districts			
Bungalow Heaven			
Historic Highlands			
Washington Square			

NORTH LAKE TIMELINE

1860-1930

- » 1860 – A burro trail is established along the route of present-day Lake Avenue, connecting Benjamin Wilson's Lake Vineyard Ranch with the mountains.
- » 1875 – Land surrounding the Lake Vineyard trail is subdivided into parcels and sold.
- » 1886 – Lake Vineyard Avenue is formally established.
- » 1888 – A Highland Railroad Company Horsecar line is established along Lake Vineyard Avenue, increasing service from Villa Street to New York Avenue.
- » 1894 – The Pacific Electric North Lake Avenue Streetcar Line is established.
- » Early 1900s – Early residential development occurs on subdivisions along Lake Avenue.
- » 1920s – Zoning districts are established, with most of North Lake Avenue designated for housing. Commercial development is initially limited to the intersections at Maple Street, Orange Grove Boulevard, and Washington Boulevard.



A Pacific Electric streetcar in Pasadena. Image courtesy of the Archives at Pasadena Museum of History (T-12-44)



The Washington Theater today

1930-1970

- » 1930 – Commercial uses are permitted to extend from Orange Grove Boulevard to Maple Street and from Claremont Street to Elizabeth Street.
- » 1930s – The Lake/Washington intersection grows as the focal point of commercial uses that serve the surrounding neighborhood.
- » 1941 – The North Lake Avenue Streetcar Line is abandoned and replaced with Bus Service.
- » 1950s – After World War II, Lake Avenue is rezoned to allow for more intensive development.
- » 1960s – Residential areas between Claremont Street and Orange Grove Boulevard are zoned for neighborhood commercial uses.



Washington Hardware Store (1403 N Lake Avenue) is constructed in 1930



Lake Avenue Church (434 Maple Way) is constructed in 1951

NORTH LAKE TIMELINE

1970-2000

- » 1970s-80s – Commercial structures increase in size and prevalence along portions of Lake Avenue, including Zody's Discounty Retailer (now CVS) in 1978 and North Lake Square Shopping Center in 1986.
- » 1970s – Pasadena's Urban Conservation Office begins surveying historic properties to comply with the Historic Preservation Act of 1976 and state mandates. City Planner John Merritt coins the term "Bungalow Heaven" to describe the predominant residential character of the area.
- » 1980s – More intense commercial uses are permitted in the area south of Mountain Street to Orange Grove Boulevard.
- » 1980s – A grassroots movement emerges to protect bungalow homes after a 1911 craftsman bungalow is demolished in favor of an apartment building.
- » 1988 – Pasadena Heritage begins hosting a Bungalow Heaven tour, with proceeds used to revitalize McDonald Park.
- » 1989 – After preparing a Conservation Plan and conducting an eight-month petition drive, the city council approves "Bungalow Heaven" as Pasadena's first landmark district.
- » 1995 – The North Lake Historic Survey is completed for the Specific Plan Area.
- » 1997 – The first North Lake Specific Plan is adopted, with a focus on establishing pedestrian-friendly and transit-oriented neighborhoods serving mixed-use business and residential districts.



North Lake Square Shopping Center (1239 N Lake Ave) is constructed in 1986

2000-present

- » 2003 – Metro A Line begins service (originally as the Gold Line) on July 26, 2003, including Lake Avenue station just south of the NLSP area.
- » 2009 – The American Planning Association names Bungalow Heaven a "Great Neighborhood" in its Great Places in America Program, referencing its human scale and outstanding architecture.
- » 2018 – The Our Pasadena Specific Plan Update process begins.
- » 2020 – The North Lake Avenue Traffic & Pedestrian Safety Enhancement Plan is initiated to provide mobility and safety enhancements to the streetscape along North Lake Avenue between Maple Street and Mountain Street.



Pasadena Accessible Apartments (915 E Rio Grande St) are constructed in 2003



Walgreens Drug Store (670 N Lake Ave) is constructed in 2006

2.2 Existing Land Uses, Urban Form, & Public Realm

LAND USE

Land use is a characterization of how a property or building is used and describes the general activity occurring on a site, such as commercial retail, office, residential, industrial, or open space. Land uses influence the surrounding environment in a variety of ways; for example, some uses, like retail stores and restaurants, may draw pedestrians to an area and create a more active sidewalk environment, while other uses, like industrial, are generally more auto-oriented in nature.

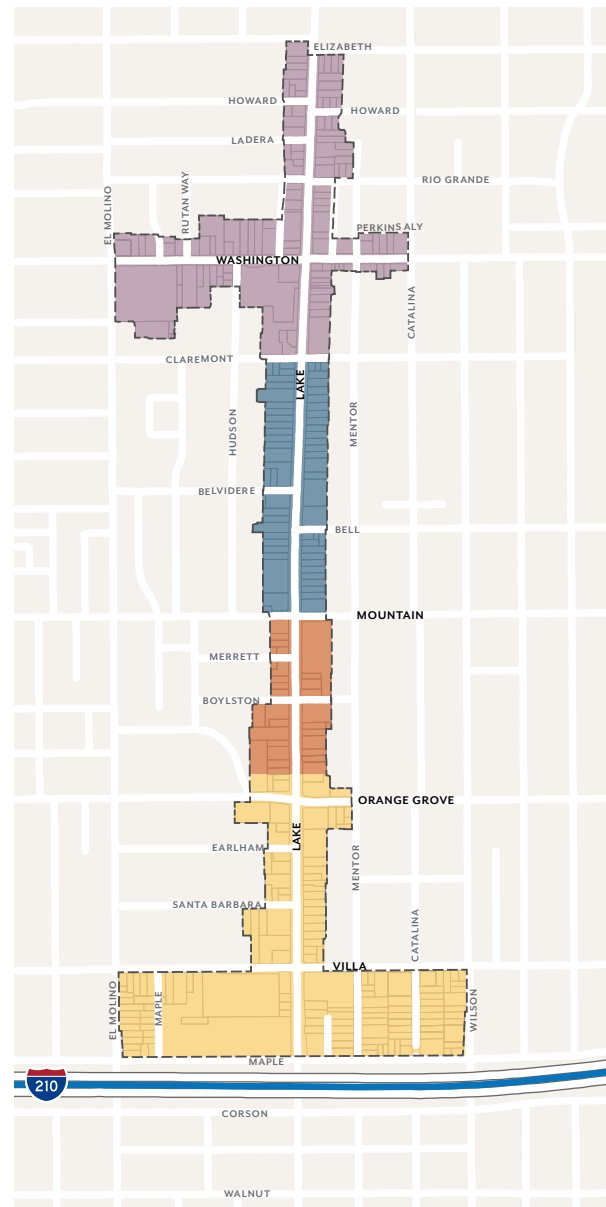
URBAN FORM

Urban form refers to the physical form of a building, both individually and collectively within a district, and its placement within a specific site. Elements of urban form such as a building's scale and height help to determine the overall character of an area. Urban form is influenced by a combination of planning regulations and development standards, architectural design, and site-specific factors such as lot size.

PUBLIC REALM

The public realm refers to spaces that are publicly owned and/or publicly accessible. The NLSP regulates the portion of the public realm between private development and the roadway, typically comprised of sidewalks, parkways, street trees, and other amenities such as seating, bicycle parking, bus shelters, and trash receptacles. Other portions of the public realm such as the roadway are designed, regulated, and maintained by various other City departments and planning documents.

Map 2.2-1: North Lake Specific Plan Subareas



- Washington Place
- Vineyard Gardens
- North Lake Village
- Lake Station District

WASHINGTON PLACE

The Washington Place subarea serves as a northern entrance to both the City and the NLSP area, extending from the Specific Plan's northern boundary at Elizabeth Street, south to Claremont Street, and along Washington Boulevard between El Molino and Catalina Avenues. The subarea is focused around the neighborhood-serving commercial node at the intersection of Lake Avenue and Washington Boulevard, with commercial office, retail, and restaurant uses occupying a variety of building typologies.

While the Santa Catalina Library, the Washington Theater building, and several traditional retail storefronts reflect the subarea's early pre-1940 development, Washington Place includes an eclectic variety of mid-to-late 20th-century and early-2000s development. Lake Avenue's somewhat fragmented and inconsistent built form reflects its competing functions as both a major thoroughfare and neighborhood-serving commercial corridor. Many commercial uses in the subarea feature parking in the rear, with sidewalk-oriented entrances that activate and provide passive surveillance to the streetscape. Other buildings have blank walls and poor transparency along the sidewalk that detract from the pedestrian-friendly design.

In contrast with the subarea's sidewalk-facing storefronts, the North Lake Square Shopping Center serves as the most prominent example of the subarea's auto-oriented strip mall developments, containing approximately three acres of surface parking on the southwest corner of Lake Avenue and Washington Boulevard. This condition reflects a stark shift in the urban form that occurred with the rise of the automobile. Buildings along the west side of Lake Avenue mirrored the contiguous, pedestrian-oriented storefront form along the east side of the street. The west side of

Lake Avenue was demolished in 1951 to accommodate the construction of the shopping center and parking lot. The pedestrian-oriented storefront form on the east side of the street remains. Similarly, the gas station on the northeast corner of the Lake/Washington intersection replaced a pedestrian-oriented commercial storefront that housed the Ice Cream Parlor, a significant community gathering space. Aside from the strip malls, and gas station, commercial buildings along Lake Avenue are typically built to the sidewalk without a setback. Residential uses in the subarea are typically single-family homes featuring substantial grass lawn setbacks of 20 feet or greater. Buildings within the subarea are generally one to two stories in height, with a few exceptions such as the three-story apartments attached to the Washington Theater building.

Like its urban form, Washington Place's public realm features a variety of conditions reflecting an evolution of planning priorities and development styles over the past century. Sidewalk widths and conditions are generally adequate for pedestrian travel, and street trees are frequent and large enough to provide visual enhancement and shade. However, the public realm offers inconsistent amenities and attention to pedestrian comfort and experience, particularly for an area with predominantly neighborhood-serving commercial uses.

Sidewalk widths in the subarea are generally 10 to 12 feet along both Lake Avenue and Washington Boulevard, with some exceptions. Sections of sidewalk on both sides of Lake Avenue between Washington Boulevard and Rio Grande Street have been extended to approximately 18 feet in width, accommodating additional space for parkways and tree plantings. Parkway widths in the subarea are typically



The Oversen Building is one of the traditional storefronts in the subarea.



An example of neighborhood-serving commercial uses along Lake Avenue.

5 feet, however some parkways along Lake Avenue north of Washington Boulevard feature irregular parkway widths of 1-3 feet, and up to 10 feet on extended sidewalk segments. Most parkways include grass, street trees, and occasional landscaping elements.

Street tree canopy in the subarea is most consistent along Lake Avenue in the block south of Washington Boulevard, where sidewalk street trees are complemented by additional street trees along a landscaped street median. Most trees planted along other portions of Lake Avenue in the subarea are mature enough to provide intermittent shade coverage on both sides of the street, however street trees and associated shade coverage is much less consistent along Lake Avenue north of Rio Grande Street, particularly on the west side of the street. Sidewalks along Washington Boulevard within the subarea are consistently planted with columns of palm trees, which do not provide shade canopy for pedestrians despite framing the street with striking visual effect.

At bus stops, riders can typically find benches and trash receptacles, however the only covered bus shelter in the subarea is located at the corner of Lake Avenue and Claremont Street. Sidewalk lighting is provided through ornately designed pedestrian-scale streetlamps. Signalized intersections offer pedestrian crossing opportunities along Lake Avenue at Rio Grande Street, Washington Boulevard, and Claremont Street. Other than the Lake Avenue intersection, there are no marked pedestrian crossing opportunities along Washington Boulevard within the NLSP area, other than El Molino Avenue at the western edge of the subarea (1,400 feet from Lake Avenue), and Catalina Avenue at the eastern edge of the subarea (750 feet from Lake Avenue).

The 5-acre Washington Park, located at the intersection of Washington Boulevard and El Molino Avenue, provides ample green space along with playground equipment, basketball and tennis courts, and a softball diamond. There are currently no other public plazas, pocket parks, or smaller public gathering spaces.



A small business on Lake Avenue at Howard Street with a mural.



Washington Park is the only public park in the subarea.



A bus shelter at the corner of Lake Avenue and Claremont Street is the only covered bus stop in the subarea.



Example of curb extensions along Lake Avenue with enhanced landscaping.

VINEYARD GARDENS

The Vineyard Gardens subarea is a nod to “Lake Vineyard Avenue,” the former name of Lake Avenue. The subarea encompasses the section of Lake Avenue between Claremont Street to the north and Mountain Street to the south. It has a predominantly residential character, comprising a mix of single-family and multi-family residential uses, however small commercial uses are also located throughout the subarea, many of which occupy formerly residential structures. The scale and form of its development reflects that of the neighboring Bungalow Heaven and Washington Square historic residential districts.

The subarea is characterized by a variety of historic architectural styles, low building heights, and large landscaped setbacks. The north/south block lengths are long and comprised of mostly small lots with approximately 45 feet of frontage and shallow depths, reflecting the early 1900s subdivisions intended for residential use. One- and two-story single-family residential buildings that remain include California Bungalow (Craftsman), Mission Revival, and Minimal Traditional styles that were popular from the 1900s through the 1940s. Many of the residential front yard setbacks were reduced from their original width to accommodate the 1929 widening of Lake Vineyard Avenue into a major arterial street.

Greater variation to the subarea’s urban form is introduced south of Belvidere Street. The most notable exception to the subarea’s design character is the Los Angeles County Social Services building, which is one of the subarea’s few three-story structures, and features high level windows and blank walls facing Lake Avenue.



A neighborhood business operating in an older, single-family structure.

The subarea’s public realm includes mature street trees that provide high-quality shade in some segments but are inconsistent, in part due to the prevalence of driveways. Sidewalk widths within the subarea along North Lake Avenue are typically between 10 to 12 feet, with approximately half of the sidewalk width dedicated to grass parkway. Decorative brick paving and street trees are incorporated in sidewalk extensions toward the southern end of the subarea, but are too inconsistent to provide to create a cohesive public realm character. Bus stops on either side of Lake Avenue at Belvidere Street provide a sheltered bench and trash receptacles. Sidewalk lighting is provided through ornately designed pedestrian-scale streetlamps.

In addition to signalized pedestrian crossings at the northern and southern boundaries of the subarea (Claremont Street and Mountain Street) the only opportunity for pedestrians to cross Lake Avenue within the subarea is at Belvidere Street, located 1,000 feet from the nearest crossing in either direction. Given Lake Avenue’s 75-foot roadway width, the lack of dedicated crossing opportunities presents accessibility challenges for pedestrians in the area.



Small commercial uses and multi-family residential uses along Lake Avenue.



Sheltered bus stop located along Lake Avenue at Belvidere Street.

NORTH LAKE VILLAGE

The North Lake Village subarea encompasses the section of Lake Avenue between Mountain Street to the north and Orange Grove Boulevard to the south. The subarea is characterized primarily by commercial retail uses, many of which are occupied by several fast food establishments with street-facing surface parking lots and drive-through facilities. The subarea also includes several neighborhood-serving small businesses, as well as a large CVS drug store and pharmacy.

The subarea is intersected by Boylston Street about halfway between Mountain Street and Orange Grove Boulevard. On the east side of Lake Avenue, this creates two large north/south blocks of approximately 600 feet each. On the west side of Lake Avenue, Merrett Drive intersects the northern half of the subarea, creating two smaller blocks. The CVS and associated surface parking lot occupy approximately 3 acres in the northeastern block of the subarea.

The subarea is characterized by 1-2 story structures that reflect an eclectic variety of design styles from mid- to late-20th Century development and a diversity of site planning arrangements with low lot coverage. The prevalence of small building footprints, inconsistent setbacks, large surface parking lots, and frequent driveway curb cuts create an auto-oriented character. Many commercial lots feature shared driveways and surface parking lots for adjacent businesses. Sidewalk-fronting storefronts generally provide surface parking lots in the rear, exemplified by Rose City Plaza's retail businesses along Lake Avenue.

Facade treatments lack consistency within the subarea. Some sidewalk-oriented buildings in the subarea contribute to visual interest and the pedestrian experience through transparency, awnings, and articulation, while other commercial buildings, most notably the CVS, feature blank walls fronting the sidewalk. Some commercial operations



One of the fast food establishments in the North Lake Village subarea.

have adaptively reused formerly residential homes, similar to those found more commonly in the Belvidere Gardens subarea. Fast-food establishments within the subarea have typically received contemporary architectural updates in line with corporate design guidance.

The Eberle Mortuary building, occupied by Union Station Homeless Services at 825 Orange Grove Boulevard, is a designated Pasadena landmark. According to the City's 1995 North Lake Historic Survey, the building appears eligible for the National Register of Historic Places as a significant example of Mission Revival architecture.

The subarea's public realm typically provides adequate sidewalk widths, however pedestrian comfort is challenged



Vehicle repair business featuring a large driveway curb cut.



CVS and its surface parking which take up 3 acres in the northeast of the subarea.



Sidewalk conditions featuring street furniture.

by development patterns that focus on vehicle travel and access for surface parking lots and drive-thru lanes. The Pasadena North Lake Avenue Traffic & Pedestrian Safety Enhancement Plan proposes a traffic signal with high visibility crosswalks and curb extensions at the central intersection of Lake Avenue and Boylston Street, which currently lacks the infrastructure for pedestrians to cross Lake Avenue safely within the subarea.

Improved sidewalks north of Boylston Street feature extended widths with decorative brick paving and large tree wells. Street tree coverage is inconsistent, however mature street trees provide shade toward the north and southern ends of the subarea. A landscaped median in the southern portion of the subarea also contains mature trees, contributing to traffic calming and greening in the subarea.

Public street furniture and amenities in the subarea are limited to bus stop benches and trash receptacles, and occasional bicycle racks. Sidewalk seating areas associated with restaurants contribute positively to the pedestrian experience and activate the public realm. Sidewalk lighting is provided through ornately designed pedestrian-scale streetlamps.



Restaurant offering outdoor seating for diners.



One of the few residential homes within the district.



View of landscaped median on Lake Avenue.



Improved sidewalks with decorative brick paving and large tree wells.

LAKE STATION DISTRICT

The Lake Station District is the NLSP's southernmost subarea, bounded by Orange Grove Boulevard to the north, Wilson Avenue to the east, Maple Street to the south and El Molino Avenue to the west. The I-210's Lake Avenue freeway on and off ramps, Lake Avenue Overpass, and Lake Metro A Line Station (accessible via the overpass) all contribute to a multimodal southern gateway to the subarea. The subarea is characterized by a mix of commercial, institutional, and residential uses. Residential and institutional uses are primarily focused in the area south of Villa Street, between El Molino Avenue and Wilson Avenue, with the prominent Lake Avenue Church and Villa Gardens health center and retirement community to the west of Lake Avenue.

Institutional uses continue up the west side of Lake Avenue to Santa Barbara Street, including Fire Station 33 and Pasadena Covenant Church. Although not an institutional use, the Pasadena Job Center serves as a resource in connecting reliable workers with homeowners and contractors, and as a community hub for education and cultural exchange. A variety of commercial uses front Lake Avenue and Orange Grove Boulevard throughout the rest of the subarea. At the time of the NLSP's update, the City acquired a parcel at the southeast corner of Lake Avenue and Villa Street (formerly Kaiser Permanente) with the intent of redeveloping the site with multifamily housing and neighborhood services.

The Lake Station District's urban form is generally characterized by one to two-story buildings with a mix of architectural characters. Notably, the subarea's institutional uses introduce exceptions to the scale and form of development, featuring taller building heights and unique design. The subarea's two long blocks on the east side of Lake Avenue reinforce an auto-oriented corridor character, while the west side of Lake Avenue is broken into four blocks, contributing to a more walkable environment and providing access to adjacent neighborhoods via Earlham Street and Santa Barbara Street.

Many commercial storefronts and office uses in the subarea contribute to a relatively consistent street wall with structures built at or near the front property line, as well as a pedestrian-oriented character with windows, awnings, and entrances that open to the street. However, many include surface parking lots and blank or opaque walls adjacent to the sidewalk.

The southern portion of the subarea also includes two residential areas between Villa Street and Maple Street: the block between El Molino Avenue and Maple Way, and the block between Mentor Avenue and Wilson Avenue. These blocks feature a mix of single-family and multi-family



I-210 freeway on ramp along Lake Avenue and the Lake Avenue Metro A Line Station.



Fire Station 33, located in the subarea.



Single family residential homes along Maple Way.

residential building typologies, typically ranging between 1 to 3 stories. Many of the subarea's single-family houses are built in the California Bungalow and Craftsman styles typical of Pasadena's historic urban form. The C.C. Lory House, a local landmark and significant example of the early 1900s Vernacular Hipped Cottage architectural style, is located in the subarea along Catalina Avenue.

The public realm within the Lake Station District subarea is adequately designed to accommodate pedestrian activity, however Lake Avenue's street design and building forms prioritize vehicle movement and access, greatly diminishing the overall pedestrian experience. Pedestrian access is particularly challenging along Lake Avenue between Orange Grove Boulevard and Villa Street, which lacks a marked pedestrian crossing opportunity for 1,200 feet. Sidewalk widths range from 10 to 12 feet throughout the corridor, with relatively consistent tree wells and streetlamps, and occasional bicycle racks. Most blocks include a bus stop with seating and a trash receptacle. A lack of parkways along the corridor maximizes room for pedestrian travel and sidewalk amenities, but increases a feeling of exposure to vehicle traffic. Some segments of the subarea have clusters of street trees planted with enough consistency to provide shade and a sense of buffer from the street, however mature street tree canopy is lacking from several portions of the corridor. Some more recent tree plantings offer potential to enhance the pedestrian experience through shade and buffering as they mature. The subarea does not include any publicly-accessible open spaces.



Bus stop with street furniture along Lake Avenue.

Public realm conditions along Villa Street, Maple Street, El Molino Avenue, Mentor Avenue, Catalina Avenue, and Wilson Avenue in the residential portions of the subarea are comfortable with consistent shade provided by mature street trees. Full sidewalk widths typically range from 10-15 feet, with 5 feet of walkable sidewalk space and 5 to 10 feet of grass parkway.

The North Lake Avenue Traffic and Pedestrian Safety Enhancement Plan proposes sidewalk improvements from Orange Grove Boulevard to Maple Street, including curb extensions at all corners of the North Lake Avenue and Orange Grove Boulevard intersection and enhanced signals, signage, and striping at the fire station. South of Villa Street, the Safety Enhancement Plan proposes traffic calming measures and improved pedestrian conditions such as high-visibility crosswalks and pedestrian refuge island to shorten crossing distance across Lake Avenue.



A historic pastrami restaurant, The Hat, located along Lake Avenue.



Medical office located along Lake Avenue.

Ch. 3

Vision, Goals & Policies

Vision, Goals & Policies	35
3.1 Vision	36
3.2 Goals & Policies.	38





Vision, Goals & Policies

CHAPTER OVERVIEW

The NLSP Vision, Goals, and Policies establish the desired outcomes of the plan and provide general direction for achieving these outcomes.

VISION

- » The vision characterizes the intended future of the NLSP area, as shaped by both the General Plan and extensive community input during the plan update process. The NLSP vision contains an overarching vision statement and seven supporting vision objectives.
- » The Vision Concept Map (Figure 3.1-1) illustrates how the vision is applied to specific areas along the corridor.

GOALS

- » A goal is a statement that describes a desired future condition or “end” state. Goals are change and outcome oriented, achievable over time, though not driven by funding.

POLICIES

- » A policy is a clear statement that guides a specific course of action for decision-makers to achieve the associated goal.

The vision, goals, and policies in this chapter are presented in the following sections:

» 3.1 Vision

- » 3.1.1 Vision Statement
- » 3.1.2 Vision Objectives

» 3.2 Goals & Policies

- » 3.2.1 Plan Area Goals & Policies
 - » Public Realm
 - » Development & Design
 - » Economic Development
- » 3.2.2 Subarea Goals & Policies
 - » Washington Place
 - » Vineyard Gardens
 - » North Lake Village
 - » Lake Station District



Towards a Great North Lake Avenue in Chapter 5 provides additional guidance on transforming North Lake Avenue into a more multimodal, people-focused boulevard with new amenities, landscaping, programming and more.

3.1 Vision

3.1.1 VISION STATEMENT

North Lake will be a vibrant and visually cohesive corridor, weaving together several distinct pedestrian-oriented districts that complement and build upon the cultural and architectural history of the community and its surrounding neighborhoods.

3.1.2 VISION OBJECTIVES

1. Sustainable Growth

A lively mix of new residential and neighborhood-serving retail shops, restaurants and services, close to transit and open space.

4. Economic Vitality

Small businesses and local institutions that contribute to the area's economy, providing jobs as well as other benefits to the area.

2. A Sense of Place

Places with a unique identity and character that people want to visit, built around engaging gathering spaces and inviting streetscapes.

5. Walkable Neighborhood

Sidewalks designed for pedestrian comfort, safety, and mobility, with easy connections to people's daily needs and multimodal transport.

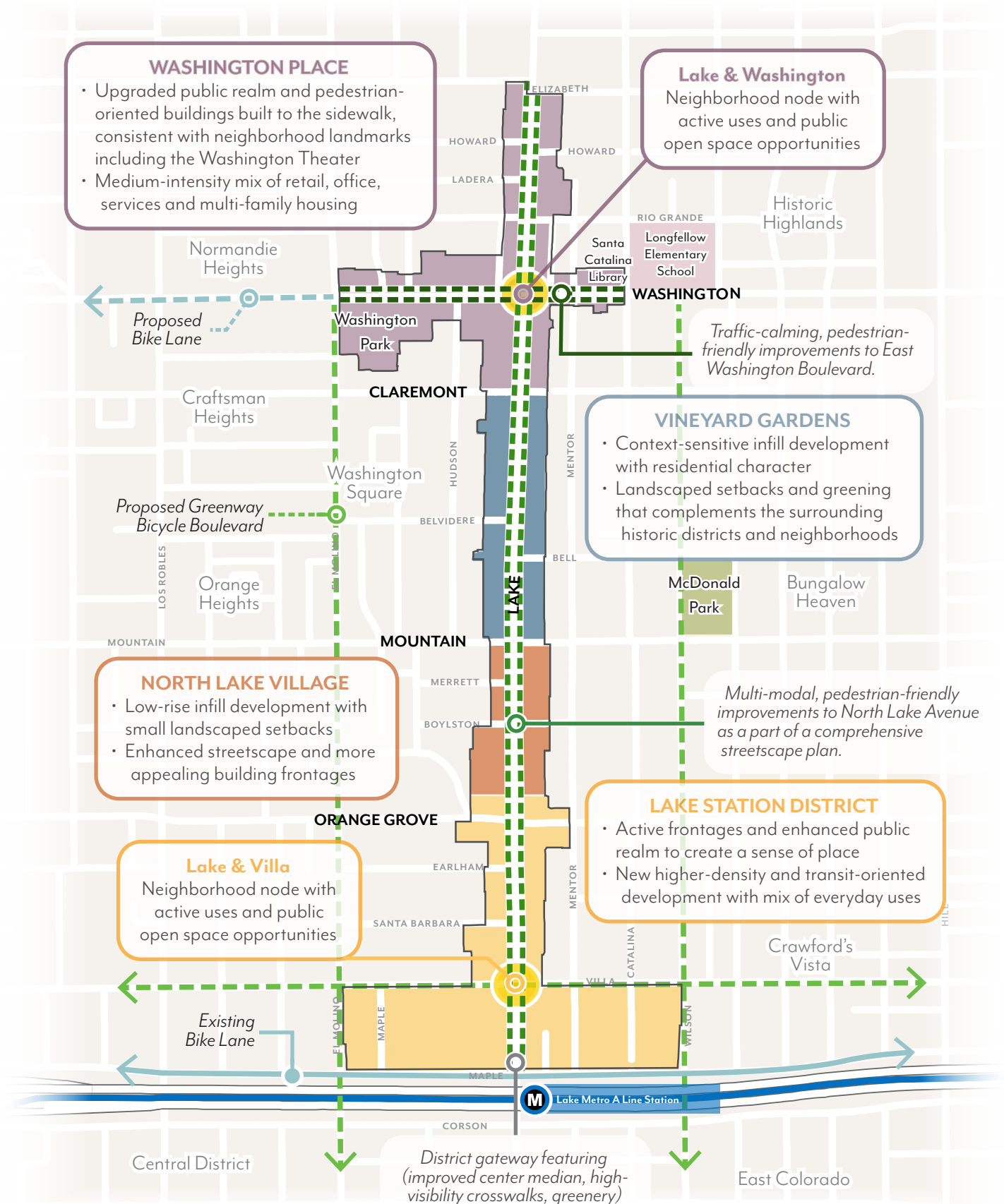
3. High-Quality Homes

Well-designed multi-family housing that creates varied and affordable options to fit all kinds of Pasadena residents and families.

6. Green Community

Attractive streets lined with consistent, healthy tree canopies and landscaping that will support climate resiliency and improve public health.

Figure 3.1-1: Vision Concept Map



3.2 Goals & Policies

The goals and policies in this section provide policy direction for implementing the vision for the NLSP area and achieving the desired outcomes based on community input and General Plan guidance. Goals and policies also provide guidance to decision makers such as City staff, City Commissions, or City Council when reviewing development projects, and they can also help support grant funding efforts to supplement the City budget for public improvement projects.

The NLSP includes goals and policies that are applicable to the entire Plan area, as well as specific subareas. The goals and policies for the Plan area are organized by topic:

- » Public Realm
- » Development & Design
- » Economic Development
- » Subareas



Sidewalks with outdoor dining, pedestrian amenities, and greening features create an active and welcoming pedestrian environment.

PUBLIC REALM

Goal 1. A high-quality public realm, including sidewalks and public open spaces, that contribute to North Lake Avenue’s overall identity and sense of place.

Policies:

- | | |
|--|---|
| <p>1.a. Walkability. Provide an unobstructed path of travel for users of all abilities that can reasonably accommodate pedestrian volumes along the corridor.</p> | <p>1.f. Activation Opportunities. Support features that attract activity and community gathering in the public realm such as outdoor dining, kiosks, seating, and public art, such as murals.</p> |
| <p>1.b. Public Amenities. Provide a designated portion of the sidewalk as the Amenity Zone for public amenities such as seating, bicycle parking, trash receptacles, bus shelters, parkways, tree wells, or other stormwater management features. Variations in design of these amenities can contribute to the distinction of subareas along the corridor.</p> | <p>1.g. Placemaking Opportunities. Introduce placemaking features such as decorative landscaping treatments, signage, and seating at North Lake Avenue’s intersections with Washington Boulevard, Mountain Street, Orange Grove Boulevard, and Villa Street, to establish continuity and enhance the corridor’s visual identity.</p> |
| <p>1.c. Enhanced Storefronts. Encourage attractive and welcoming storefronts using street-oriented entrances, planters, seating, and other enhancements to a building’s entrance and ground floor exterior.</p> | <p>1.h. Enhanced Gateways. Enhance visual and physical conditions surrounding North Lake’s gateways from areas to the north and south.</p> |
| <p>1.d. Outdoor Dining. Encourage a vibrant outdoor dining environment for restaurants fronting North Lake Avenue to generate activity and promote a lively and dynamic sidewalk zone.</p> | <p>1.i. Public Art. Encourage collaboration between City departments, Arts and Culture Commission, Northwest Commission, and local businesses to identify locations for public art installations and other aesthetic improvements that reflect and build upon the North Lake Avenue community identity.</p> |
| <p>1.e. Lighting. Provide adequate pedestrian-scale lighting along sidewalks and in public spaces to enhance the safety and comfort of visitors, especially at night, using dark sky-friendly design to limit light pollution by directing lighting downwards.</p> | |

COMMUNITY PRIORITIES FOR THE PUBLIC REALM

The NLSP Public Realm goals and policies were crafted to align with the Planning Commission Subcommittee’s vision for North Lake Avenue (introduced in Chapter 1), which could be further refined in a comprehensive streetscape plan for the corridor. A new streetscape plan could include detailed guidance and precise recommendations of improvements that would promote pedestrian strolling and discovery, connect neighborhoods, differentiate subareas, and accommodate activities such as people watching, street vending, farmers markets, and outdoor dining. The streetscape plan can also influence canopy trees, parklets, and kiosks that would provide shade and gathering spaces along the street, with street furniture, pedestrian-scaled lighting, bus shelters, bike racks, public art, and additional amenities. *Refer to Public Realm Standards (Chapter 5) and Implementation (Chapter 7).*



Sidewalks and public spaces with seating, trash and recycling receptacles, parkways, shade trees, and pedestrian infrastructure provide a comfortable and amenity-rich experience for pedestrians in commercial and mixed-use settings.



Landscaped parkways and setbacks and consistent shade trees with large, healthy canopies provide a more comfortable pedestrian experience and support sustainability objectives.

PUBLIC REALM

Goal 2. A comfortable and well-connected NLSP area that encourages sustainable modes of travel such as walking, biking, rolling, and public transit.

Policies:

- 2.a. Multi-Modal Environment.** Encourage non-driving modes of travel and multi-modal connections within the district by providing sufficient space for installations such as bus shelters and bicycle racks.
- 2.b. Traffic Calming.** Support public realm and traffic calming improvements that promote safe, comfortable, multi-modal mobility along North Lake Avenue, such as the North Lake Avenue Traffic & Pedestrian Safety Enhancement Plan.
- 2.c. Pedestrian Environment.** Improve pedestrian conditions throughout the Plan area through expanded sidewalks, more streetscape amenities and shade, and pedestrian-oriented design to enhance walkability.
- 2.d. Pedestrian Mobility.** Support the addition of signalized crossings, bulb-outs, continental crosswalks, scramble crosswalks, pedestrian lighting, and other improvements that promote safe, efficient, and comfortable pedestrian connections.
- 2.e. Bicycle Connections.** Support proposed bicycle facilities within and surrounding the Plan area per the City's Bicycle Transportation Action Plan (BTAP), Active Transportation Plan, and/or other City plans.
- 2.f. Transit Connections.** Support North Lake Avenue's connections to transit and neighboring activity hubs through addition of bicycle and pedestrian safety improvements around the Metro A Line Lake Station and freeway overpass between the NLSP and the CDSP areas.
- 2.g. Bus Stop Amenities.** At all bus stops, support implementation of shading devices to shelter transit users from the elements. Provide adequate lighting, comfortable seating, and real-time transit information.



Enhanced crossing treatments, such as a scramble, can support a safe and comfortable pedestrian experience.



Wide sidewalks with tree canopies and storefront awnings that provide shade for passersby.

PUBLIC REALM

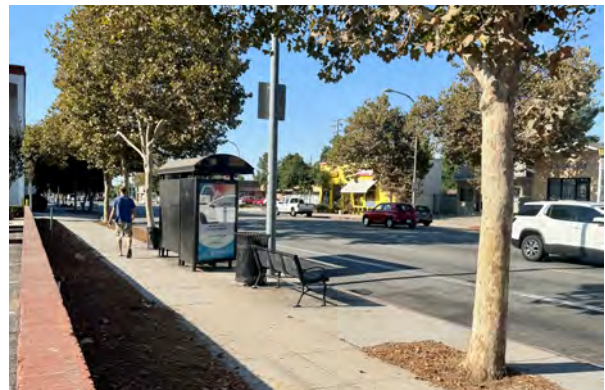
Goal 3. A green corridor with sufficient landscaping and shade coverage to encourage pedestrian mobility and support sustainability objectives such as carbon sequestration, mitigating the urban heat island effect, and increasing stormwater capture.

Policies:

- 3.a. Parkway.** Incorporate parkways into the public sidewalk, providing opportunities for street tree planting, improving permeability for rain and stormwater capture, and cooling the sidewalk environment.
- 3.b. Street Trees.** Encourage street tree planting in amenity zones, street medians, and “tree lanes” to provide a substantial year-round shade canopy, and consider the addition of accent trees to support resilience and celebrate the City’s climate and heritage.
- 3.c. Street Tree Distribution.** Increase the frequency and consistency of canopy trees to improve air quality and allow pedestrians to walk the length of the corridor in a shaded environment.
- 3.d. Tree Protection.** Require the protection and maintenance of mature and healthy trees which bring aesthetic, environmental, and economic benefits to the NLSP area through the Citywide Tree Protection Ordinance.
- 3.e. Landscaped Setbacks.** Incorporate thoughtful landscaping with sustainable and native plant materials in areas where wider, buffered setbacks are appropriate.
- 3.f. Landscaped Medians.** Support the addition of landscaped street medians along the entire Lake Avenue corridor within the NLSP area to provide traffic calming benefits, safe crossing options, and opportunities for usable space such as community gardens and pedestrian ‘ramblas’.
- 3.g. Resilient Plantings.** Support the inclusion of native, drought-tolerant and fire-resistant species to increase urban resilience and create habitat for insects and birds.
- 3.h. Water Conservation.** Encourage low-impact development (LID) and smart irrigation techniques in all landscaping, incorporating permeable paving, bio-swales, and other methods to control runoff and capture stormwater for onsite reuse.



Parkways provide opportunities for street tree plantings, landscaping, and improving permeability.



Consistent street trees add beauty to the public realm and improve the pedestrian experience.

DEVELOPMENT & DESIGN

Goal 4. Complementary building forms that reflect the scale of the neighborhood, preserve historic resources, and support a vibrant, walkable district.

Policies:

- 4.a. Transit-Oriented Development.** Promote higher density, transit-oriented development within close proximity of the Metro A Line station, supported by multi-modal linkages, and pedestrian and open space amenities.
- 4.b. Architectural Diversity.** Allow for a range of architectural styles and forms that provide visual interest and quality design through massing and façade standards that strengthen the corridor's identity.
- 4.c. Historic Compatibility.** Require new development adjacent to the district's five designated historic districts and various designated properties to be compatible with historic scale and design features.
- 4.d. Scaled Transitions.** Require higher-intensity new development along North Lake Avenue to provide appropriate transitions to adjoining lower density residential neighborhoods.
- 4.e. Transparency.** Require façade transparency to improve visual interest, reduce the prevalence of blank walls, and establish a connection between ground floor commercial uses and the public realm.
- 4.f. Pedestrian Orientation.** Support site planning, façade design, and landscaping to support pedestrian-oriented places that focus activity on the street.
- 4.g. Materials.** Use durable, high-quality materials that exhibit a sense of permanence on all sides of a building, but particularly at the street level and entrances.
- 4.h. Hardening.** Employ materials and designs that reduce the chance of ignition during wildfires, to include using flame-resistant siding and roofing, enclosing eaves, covering vents, and clearing flammable material from a perimeter buffer zone.
- 4.i. Loading, Storage, and Utilities.** Locate service areas away from public streets and residential neighborhoods, as far as feasible.
- 4.j. Parking Impacts.** Locate onsite parking underground or in the rear of the lot with access from a secondary street to reduce the visual impact and prevent the introduction of new curb cuts or new surface parking lots abutting North Lake Avenue.



Ground floor design can contribute to the connection between public and private realms.



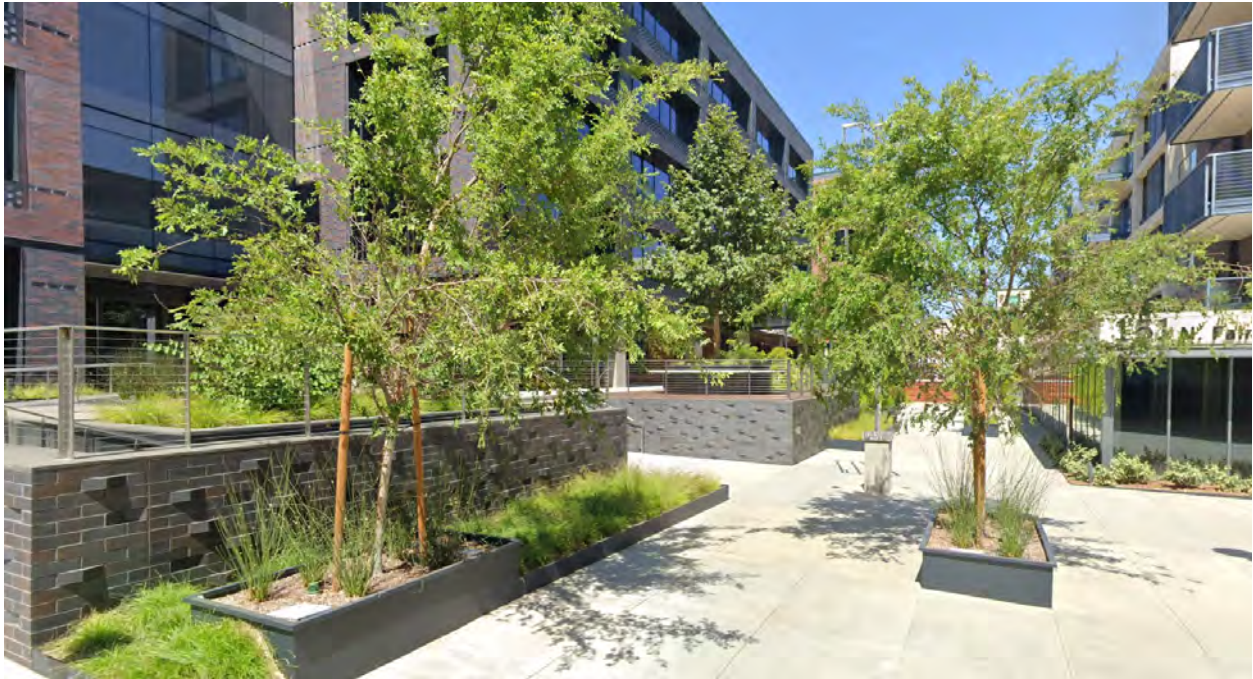
Transparency along streets establishes a connection between the public realm and private uses.

DEVELOPMENT & DESIGN

Goal 5. Ample access to open space for both passive and active enjoyment, with a range of well-designed private, common, and public spaces.

Policies:

- 5.a. Residential Open Space.** Incorporate private and common open space areas that correlate to a building's size and number of residents, including sheltered places for people to sit and talk such as porches, loggias, balconies, and arcades.
- 5.b. Commercial Open Space.** Require large non-residential or mixed-use projects to provide open space for employees, visitors, and the public.
- 5.c. Functional Spaces.** Create usable, functional open spaces for residents, employees, and visitors alike.
- 5.d. Urban Greening.** Use open space areas to further environmental goals – such as carbon sequestration and reducing the urban heat island effect – through tree planting, stormwater capture, and native landscaping.
- 5.e. Comfortable Setting.** Create a pleasant micro-climate in open space areas, including shade from summer sun and access to winter sunlight; locate seating with consideration to noontime sun and shade.
- 5.f. Underutilized Spaces.** Encourage the creative transformation of underutilized spaces along the North Lake corridor, such as large surface parking lots, to add a variety of placemaking and open space opportunities.
- 5.g. Outdoor Dining.** Encourage outdoor dining along the street frontage that is protected from vehicles through methods such as screening or landscaping.



Publicly accessible open spaces such as a pedestrian paseo contribute to an enhanced public realm and create an inviting gateway environment for residents and visitors.

ECONOMIC DEVELOPMENT

Goal 6. A supportive environment for new development and businesses that are compatible with surrounding residential uses and historic resources.

Policies:

- 6.a. Healthy Mix of Uses.** Expand convenient access to transit, institutions, services, and amenities that will attract and retain businesses/private investment in the community.
- 6.b. Redevelopment Potential.** Incentivize redevelopment of the underutilized Food 4 Less, CVS, and former Big Lots sites to replace large surface parking lots with new housing above community serving stores or offices along the North Lake Avenue frontage.
- 6.c. Housing Options.** Support a range of housing types, including co-living and micro-units, that allow for variety in affordability and configurations, provide shared amenities, and are suitable for people across all phases of life.
- 6.d. Adaptive Reuse.** Encourage adaptive reuse to support new businesses and character of existing buildings.
- 6.e. Balanced Requirements.** Elevate quality of design while maintaining a reasonable level of flexibility to increase market feasibility for new developments.
- 6.f. Parking Reductions.** Reduce parking requirements to incentivize historic preservation, the provision of public open space, transit usage, and to lower the cost of development.
- 6.g. Lower Barriers to Entry.** Simplify parking standards and exempt small businesses from costly parking requirements to enable greater flexibility for changing uses and economic factors.
- 6.h. Park Once Districts.** Support opportunities to “Park Once and Walk” along North Lake Avenue by working with property and business owners to increase shared parking in publicly owned parking lots and to reduce on-site parking requirements.
- 6.i. Façade Improvements.** Encourage and assist existing owners to upgrade their properties flanking the public right-of-way through building repair, re-modeling, re-painting, and landscaping.
- 6.j. Cohesive Business District.** Develop and maintain a cohesive and unique visual identity for the NLSP area’s businesses, supported by formation of a business and/or property-owner association and comprehensive sign program.



Adaptive reuse can preserve the character of historic residential buildings while adding convenient access to neighborhood services and a variety of other uses.



Facade treatments utilizing high-quality, textured materials and landscaping contribute to the human scale of the street.

3.2.2 SUBAREA GOALS & POLICIES

WASHINGTON PLACE

Goal 7. An active, walkable, neighborhood-serving gateway to North Lake Avenue, with development that respects and reinforces the neighborhood's rich history.

Policies:

- 7.a. Northern Gateway.** Introduce plazas and active commercial uses that create a sense of place and welcome residents and visitors into the North Lake corridor from neighborhoods to the north.
- 7.b. Historic Commercial Character.** Reinforce the historic pedestrian-oriented commercial character of the Lake/Washington intersection.
- 7.c. Enhanced Public Realm.** Enhance public realm by requiring wider sidewalks, shade trees, publicly-accessible open space, and sidewalk dining that support a sense of place.
- 7.d. Pedestrian Improvements.** Support the addition of marked crosswalks and related infrastructure that reduces crossing distances, slows vehicle speeds, and improves pedestrian connectivity and safety along East Washington Boulevard.
- 7.e. Engaging Ground Floor.** Flexible ground floor uses supported by active streetscapes and design treatments that engage pedestrians and encourage business activity.
- 7.f. Proximity to Services.** Allow for neighborhood businesses that provide daily services within walking distance for residents in the Plan area and surrounding neighborhoods.
- 7.g. Sensitive Transitions.** Allow building heights that support commercial development and provide appropriate transitions to adjacent Historic Landmark Districts such as Historic Highlands, Bungalow Heaven, and Washington Square.



Traditional architecture helps reflect the area's historic character.



Seating and landscaping, along with other amenities such as bicycle parking, shade trees, and enhanced crosswalks, provide for a comfortable pedestrian experience.



Conceptual illustrations of streetscape improvements on North Lake Avenue near Washington Boulevard.

VINEYARD GARDENS

Goal 8. A residential mixed-use corridor that encourages traditional architectural forms and details behind landscaped setbacks, reinforcing the historic character of the existing homes and adjacent neighborhoods.

Policies:

- 8.a. Architectural Diversity.** New development should complement the existing architecture and reference architectural styles such as California Bungalow, Mission Revival, and Minimal Traditional styles.
- 8.b. Historic Resources.** Preserve architecturally and culturally significant structures and encourage future alterations or repairs to maintain the resource's character-defining features.
- 8.c. Adaptive Reuse.** Encourage sensitive reuse of existing buildings to achieve the preservation and rehabilitation of both designated and undesignated historic properties.
- 8.d. Sensitive Transitions.** Require that new development along North Lake Avenue provides appropriately scaled transitions to adjoining lower density residential neighborhoods, including Historic Landmark Districts of Bungalow Heaven and Washington Square.
- 8.e. Enhanced Streetscape.** Increase setback area to provide additional sidewalk, amenities, aesthetic enhancements to activate the public realm and transition to residential uses.
- 8.f. Greening.** Contribute to overall greening along the corridor through consistent tree canopies, well-maintained parkways and landscaped setbacks.



Vineyard Gardens will continue to accommodate adaptive reuse to achieve preservation of the neighborhood's historic character while welcoming new businesses and services.



New development can complement existing architecture while introducing new types of housing for a diverse group of residents.



Conceptual illustrations of new multi-family residential development with a landscaped median on North Lake Avenue.

NORTH LAKE VILLAGE

Goal 9. A commercial mixed-use corridor supported by an enhanced streetscape that promotes opportunities for gathering spaces and neighborhood connections, designed with sensitivity to the adjacent landmark districts.

Policies:

- 9.a. Mixed-use Redevelopment.** Allow mixed-use projects in the area to expand housing opportunities and promote the revitalization of underutilized land.
- 9.b. Ground Floor Commercial.** Commercially focused ground floor uses to support existing businesses, encourage new businesses and promote more neighborhood services for the surrounding community.
- 9.c. Sensitive Transitions.** Require that commercial and mixed-use development along Lake Avenue provides appropriately scaled transitions to adjoining lower density residential neighborhoods, including the Bungalow Heaven Historic District.
- 9.d. Street Orientation.** Require new buildings to orient frontages to the street and sidewalk, phasing out auto-oriented site layouts that prioritize vehicle access and encourage driving.
- 9.e. Ground Floor Design.** Implement design standards and guidelines relating to entrances, transparency, and other elements that help create a ground floor commercial character.
- 9.f. Outdoor Dining.** Encourage the re-use and redesign of outdoor space such as excess surface parking areas to establish a vibrant and inviting outdoor dining environment.
- 9.g. Greening.** Contribute to overall greening along the corridor through consistent tree canopies and well-maintained parkways.
- 9.h. Pedestrian Improvements.** Support implementation of the City's North Lake Traffic and Pedestrian Safety Enhancement Plan through sidewalk improvements and high visibility pedestrian crossings.



Conceptual illustration of pedestrian-oriented mixed-use development and streetscape enhancements.



Introducing outdoor dining and seating in leftover areas and those previously used for surface parking enhances the public realm and pedestrian experience.



Street-oriented building frontages, outdoor dining, and plants create a welcoming streetscape that enhance the sidewalk and benefit businesses by increasing foot traffic.

LAKE STATION DISTRICT

Goal 10. A higher-intensity mix of retail, office, and multi-family housing uses that create a vibrant, transit-oriented southern gateway to the North Lake Avenue corridor.

Policies:

- 10.a. Transit Village.** Support higher density residential and commercial development that encourages transit use and provides new housing opportunities.
- 10.b. Southern Gateway.** Introduce wider sidewalks, trees, and active uses that create a sense of place and welcome residents and visitors into the North Lake corridor from the A Line Station and Central District to the south.
- 10.c. Quality Public Spaces.** Encourage the development of publicly-accessible open space, such as paseos, in the subarea, particularly near the intersection of North Lake Avenue and Villa Street.
- 10.d. Pedestrian Improvements.** Support implementation of the City's North Lake Traffic and Pedestrian Safety Enhancement Plan through sidewalk improvements and high visibility pedestrian crossings.
- 10.e. Small Setbacks.** Minimize building setbacks to establish a street wall and use any setback area to provide additional sidewalk, amenities, or aesthetic enhancements to activate the public realm.
- 10.f. Ground Floor Design.** Incorporate design features that help activate the ground floor and create a connection between the public and private realms.
- 10.g. Sensitive Transitions.** Balance higher-intensity commercial development standards with stepbacks that provide appropriate transitions to adjacent residential properties.
- 10.h. Greening.** Contribute to overall greening along the corridor through consistent tree canopies and locating driveways away from North Lake Avenue.



Higher density residential and commercial development is allowed in the subarea near transit.



Publicly-accessible open space can improve the public realm with the addition of landscaping and trees.



Conceptual illustrations of new mixed-use buildings along North Lake Avenue with publicly-accessible open space.

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Ch. 4

Zoning & Land Use

Zoning & Land Use	61
4.1 Applicability	61
4.2 Zoning Districts	62
4.3 Allowable Land Uses.	64





Zoning & Land Use

CHAPTER OVERVIEW

The zoning and land use regulations in this chapter are intended to guide development and decision making to achieve the vision of the NLSP. While broad land use categories are assigned in the General Plan, the NLSP establishes a detailed list of allowed land uses and permit requirements for each zoning district within the plan area.

This chapter is organized into the following sections:

- » **4.1 Applicability**
- » **4.2 Zoning Districts**
- » **4.3 Allowable Land Uses**



Commercial Use



Residential Use



Institutional Use

4.1 Applicability

The applicability of NLSP land use regulations and development standards are organized by zoning district and plan chapters (Table 4.1-1). Where the standards do not apply, the relevant section of Pasadena Municipal Code (PMC) is referenced.

Table 4.1-1: Applicable Specific Plan Chapters

Zone	Specific Plan Chapters			
	Vision, Goals & Policies	Zoning & Land Use	Public Realm Standards	Development Standards
	3	4	5	6
NL-MU-C	✓	✓	✓	✓
NL-MU-N	✓	✓	✓	✓
NL-RM-48	✓	17.22	✓	17.22
NL-RM-32	✓	17.22	✓	17.22
NL-RS-6	✓	17.22	✓	17.22
OS	✓	17.26	✓	17.26
PS	✓	17.26	✓	17.26



Open Space

4.2 Zoning Districts

The purpose of the NLSP zoning districts (Map 4.2-1) is to implement the plan vision for each of the subareas as described in the NLSP document, and summarized below.

NL-MU-C

Mixed-Use Core

- » Create mixed-use activity centers that encourage a range of active uses where people can walk to shops, restaurants, and jobs in proximity of transit or major intersections
- » Support projects that are entirely commercial, entirely residential, or mixed-use, with ground floor commercial required at key intersections

NL-MU-N

Mixed-Use Neighborhood

- » Promote the development of pedestrian-friendly neighborhoods with commercial uses that are sensitive to neighboring residents
- » Support projects that are entirely commercial, entirely residential, or mixed-use, integrated either horizontally or vertically consistent with ground floor use requirements

NL-RM-48/32

Residential Multi-family

- » Maintain the current uses and protect the existing residential character using City of Gardens standards

NL-RS-6

Residential Single-family

- » Provide areas for low density residential neighborhoods and relate new development to the existing environment

OS

Open Space

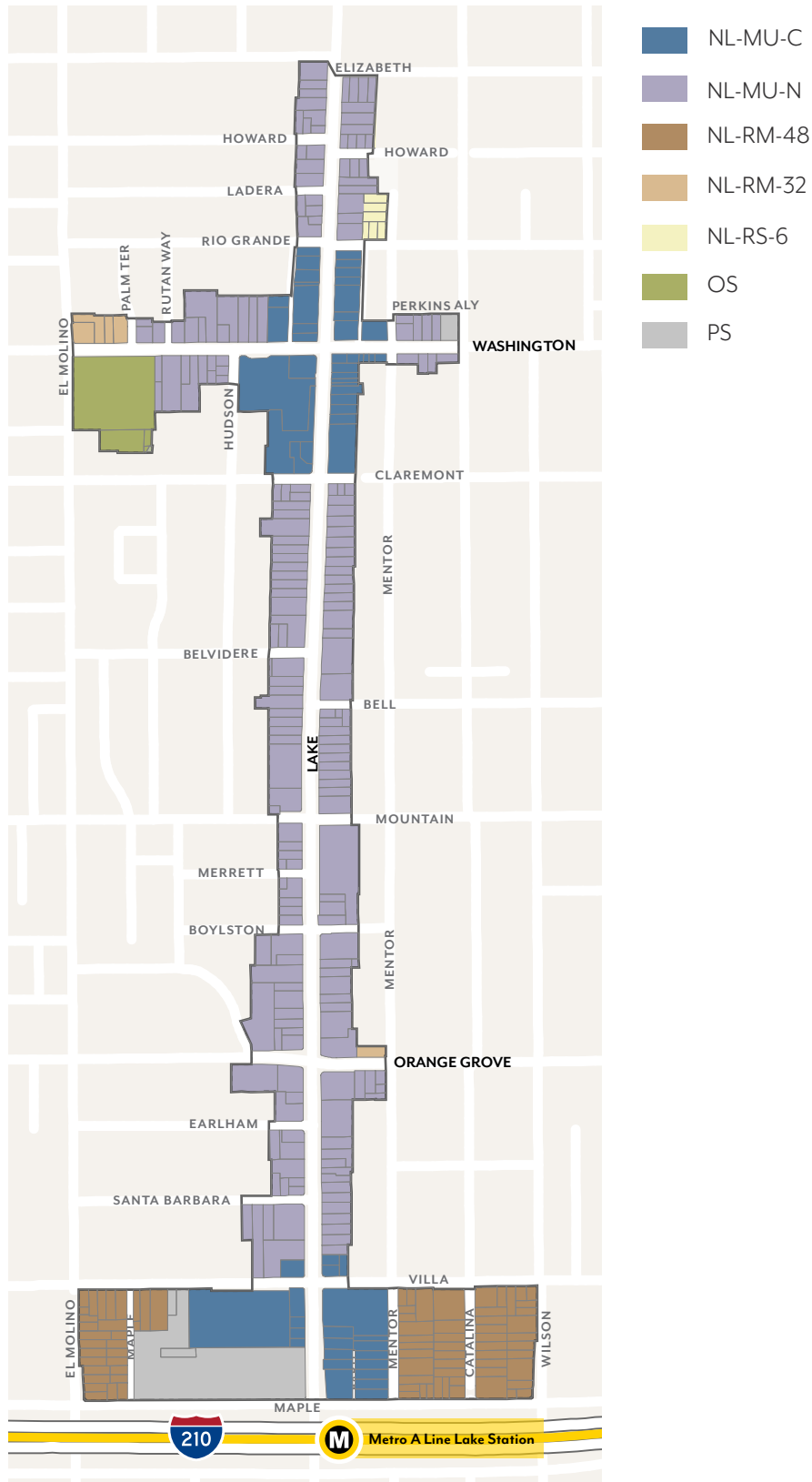
- » Provide opportunities for parks and recreation to residents and visitors

PS

Public Semi-public

- » Provide for large public or semi-public land uses that may not be appropriate in other base zoning districts

Map 4.2-1: Zoning Districts



4.3 Allowable Land Uses

- A. **Definitions.** Definitions of specific land uses are found in PMC 17.80.020.
- B. **Permit Requirements.** Table 4.3-1 identifies the uses of land allowed, the land use permit required to establish each use, and limitations that may apply for a particular use.
- C. **Standards for Specific Land Uses.** Additional standards may apply to specific land uses; refer to the PMC section noted in the table, except as noted below.
 - 1. PMC 17.50.160 shall not apply to Mixed-Use Projects.
 - 2. PMC 17.50.350 shall not apply to Multi-Family Housing.
- D. **Ground Floor Frontages.** In Mixed-Use zoning districts, additional commercial requirements and residential unit restrictions on the ground floor shall apply per Section 6.2.1.
- E. **Major Construction.** For all non-residential uses with a gross floor area of 25,000 square feet or greater, a Conditional Use Permit shall be required per PMC 17.61.050.J.2.
- F. **Prohibited Uses.** Those uses not listed in Table 4.2-1 are prohibited by this Specific Plan, except as otherwise provided by PMC 17.21.030.A.
- G. **Nonconforming Uses.** Existing uses which are made nonconforming by this Specific Plan shall be subject to PMC 17.71, with the following exceptions:
 - 1. The following uses shall not be expanded:
 - a. Vehicle Services (except Automobile Rental)
 - b. Commercial Off-Street Parking
 - 2. A drive-through business may be remodeled, but not enlarged, with the approval of a Minor Conditional Use Permit. The number of queueing positions may be increased with the approval of a Minor Conditional Use Permit. The site of a drive-through business shall not be expanded onto another property.

Table 4.3-1: Allowable Land Uses

Symbol	Description	PMC Section
P	Permitted use, Code Compliance Certificate required.	17.61.020
MC	Conditional use, Minor Conditional Use Permit required.	17.61.050
AMC	Conditional use, Administrative Minor Conditional Use Permit required	17.61.050
C	Conditional use, Conditional Use Permit required.	
AC	Conditional use, Administrative Conditional Use Permit required	
E	Conditional use, Expressive Use Permit required.	17.61.060
TUP	Temporary use, Temporary Use Permit required.	17.61.040
—	Use not allowed.	

LIMITATIONS:

(L1) Use is not permitted on the ground floor within 35 feet of the sidewalk line. Entries to upper/lower floors and ground floor spaces behind 35 feet are allowed; these spaces shall not qualify as required commercial uses for the purposes of Section 6.2.1.

(L2) Use is limited to a maximum of 30% of the total building frontage on the ground floor, regardless of the ground floor frontage type per Section 6.2.1.

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENTS			
Land Use ¹	Permit Requirement		PMC Section
	NL-MU-C	NL-MU-N	
RESIDENTIAL USES			
Accessory Dwelling Unit	P	P	17.50.275
Boarding Houses ²	P	P	
Dormitories	P	P	
Fraternities / Sororities	P	P	
Home Occupations	P	P	17.50.110
Mixed-Use Projects	P	P	
Multi-Family Housing	P	P	
Residential Accessory Uses and Structures	P	P	17.50.250
Single-Room Occupancy	P	P	
Supportive Housing	P	P	
Transitional Housing	P	P	
COMMERCIAL USES			
RECREATION, EDUCATION & PUBLIC ASSEMBLY USES			
Clubs, Lodges, Private Meeting Halls	C	C	
Colleges, Nontraditional Campus Setting	P	P	
College, Traditional Campus Setting	P	—	
Commercial Entertainment	E	—	17.50.130
Commercial Recreation, Indoor	P	P	
Commercial Recreation, Outdoor	C	C	
Conference Centers	C (L2)	—	
Cultural Institutions	P	P	
Electronic Game Centers	P	P	17.50.100
Park and Recreation Facilities	P	P	
Religious Facilities	P	P	17.50.230
with Affordable Housing	P	P	
with Columbarium	MC	MC	
with Temporary Homeless Shelter	MC	MC	
Schools, Public and Private	—	C	17.50.270
Schools, Specialized Education and Training	P	P	

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENTS			
Land Use ¹	Permit Requirement		PMC Section
	NL-MU-C	NL-MU-N	
OFFICE, PROFESSIONAL & BUSINESS SUPPORT USES			
Automated Teller Machines (ATMs)	P	P	17.50.060
Banks and Financial Services	P	P	
with Walk-Up Services	P	P	17.50.060
Business Support Services	P	P	
Offices, Accessory	P	P	
Offices, Administrative Business Professional	P	P	
Offices, Government	P	P	
Offices, Medical	P	P	
Research and Development	P	P	17.50.240
Work/Live Units	P	P	17.50.370
RETAIL SALES			
Alcohol Sales, Beer and Wine	C	C	17.50.040
Alcohol Sales, Beer and Wine at Restaurants (including fast food)	AC	AC	
Alcohol Sales, Full Alcohol	C	C	
Alcohol Sales, Full Alcohol at Restaurants (including fast food)	AC	AC	
Animal Retail Sales	P	P	
Bar / Taverns	C	C	17.50.040, 17.61.050.J
with Live Entertainment	C	C	17.50.130
Commercial Nurseries	—	C	17.50.180
Convenience Stores	P	P	
Food Sales	P	P	
Internet Vehicle Sales	P	P	
Liquor Stores	C	C	17.50.040, 17.61.050.J
Restaurants, Fast Food	P	P	17.50.260
Restaurants, Formula Fast Food	P	P	
Restaurants	P	P	17.50.260, 17.61.050.J
with Limited Live Entertainment	P	P	
with Walk-Up Window	MC (A)	MC	

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENTS

Land Use ¹	Permit Requirement		PMC Section
	NL-MU-C	NL-MU-N	
Retail Sales	P	P	
Significant Tobacco Retailers	C	C	17.50.330, 17.61.050.J
Vehicle Services, Sales, and Leasing	C	C	17.50.360
Vehicle Services, Sales and Leasing, Limited	P	P	
Vehicle Services, Service Stations	C	C	
SERVICES			
Adult Day Care, Limited	P (LI)	P	
Animal Boarding	P	P	
Animal Services, Grooming	P	P	
Animal Services, Hospitals	P	P	17.50.050
Catering Services	P	P	
Charitable Institutions	P	P	
Child Day Care Centers	—	P	17.50.080
Child Day Care, Large	P	P	
Child Day Care, Small	P	P	
Emergency Shelter	MC	MC	
Emergency Shelters, Limited	P	P	17.50.105
Laboratories	—	P	
Lodging, Bed and Breakfast Inns	—	C	17.50.140
Lodging, Hotels and Motels	C	C	17.50.150
Massage Establishments	C	C	17.50.155
Mortuaries / Funeral Homes	—	P	
Neighborhood Gardens	P	P	
Personal Improvement Services	P	P	
Personal Services	P	P	
Printing and Publishing	C	C	
Printing and Publishing, Limited	P	P	
Public Safety Facilities	C	C	
Vehicle Services, Automobile Rental	C	C	

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENTS			
Land Use ¹	Permit Requirement		PMC Section
	NL-MU-C	NL-MU-N	
INDUSTRY, MANUFACTURING & PROCESSING USES			
Alcohol Beverage Manufacturing	P	C	17.50.040, 17.61.050.J
with Accessory Tasting Room	MC	C	
Custom Manufacturing / Artisan Production	P	P	
Industry, Restricted	MC	MC	
TRANSPORTATION, COMMUNICATIONS & UTILITY USES			
Accessory Antenna Array	P	P	
Alternative Fuels/Recharging Facilities	P	P	
Communications Facilities	C	C	
Heliports	C	—	
Transportation Terminals	C	C	
Utility, Major	C	C	
Utility, Minor	P	P	
Wireless Telecom Facilities, Major	C	C	17.50.310
Wireless Telecom Facilities, Minor	MC	MC	
Wireless Telecom Facilities, SCL	P	P	
TEMPORARY USES			
Filming, Long-term	MC	MC	
Filming, Short-term	P	P	
Personal Property Sales	P	P	17.50.190
Seasonal Merchandise Sales	P	P	17.50.180
Street Fairs	P	P	
Tents	TUP	TUP	17.50.320
Other Temporary Uses	TUP	TUP	

NOTES:

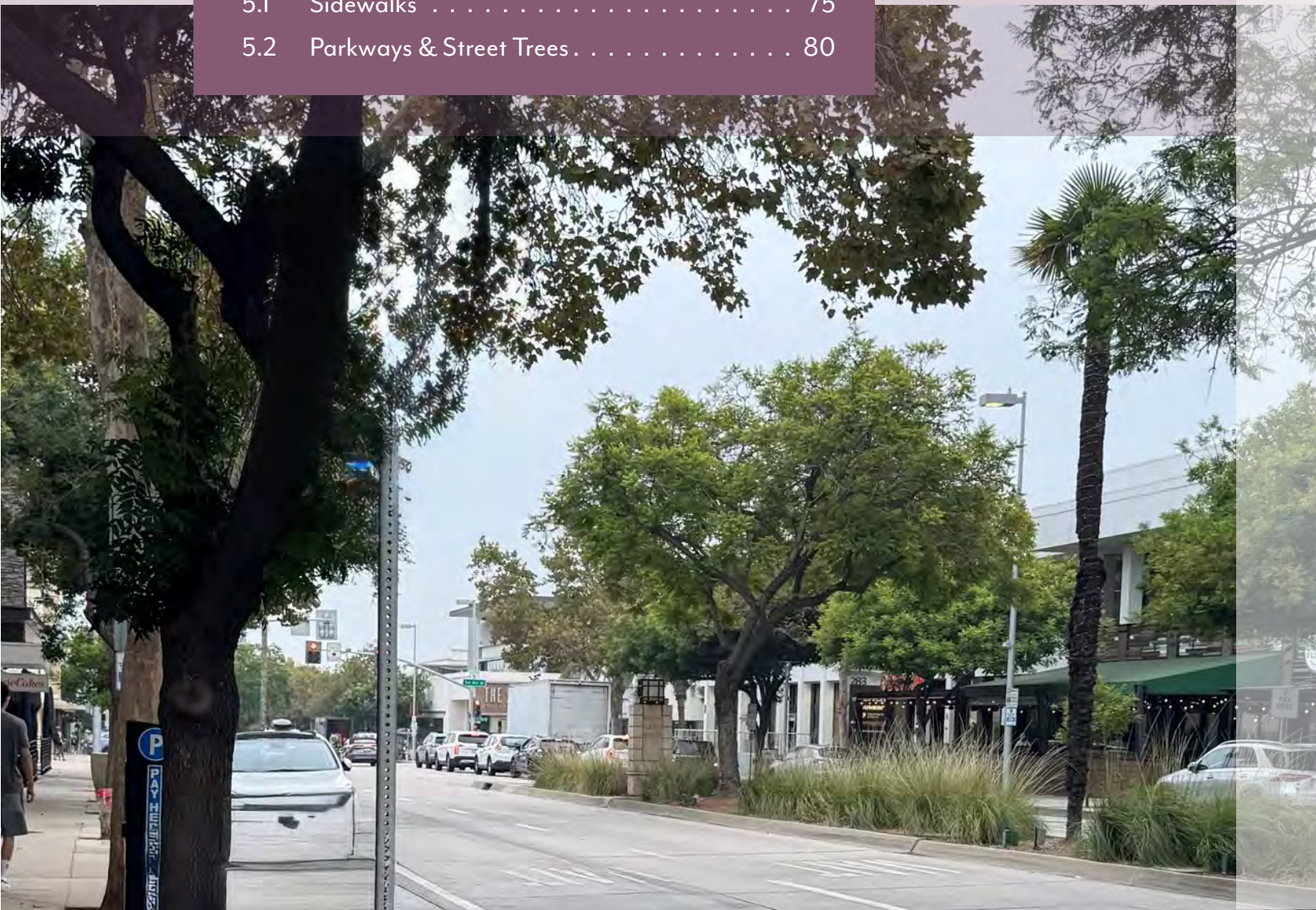
¹ See PMC 17.80.020 for definition of the listed land uses.

² Includes Co-living facilities, which may include more than one shared kitchen per building. Separation requirements of PMC 17.50.065 shall not apply.

Ch. 5

Public Realm Standards

Public Realm Standards.	71
5.1 Sidewalks	75
5.2 Parkways & Street Trees	80





Public Realm Standards

CHAPTER OVERVIEW

The public realm standards and design guidelines in this chapter serve to implement the General Plan vision for the NLSP area and achieve objectives of the Pasadena Street Design Guide, Pasadena Pedestrian Plan, and Pasadena Master Street Tree Plan. To improve the public realm for users of all abilities, and to provide enough space for simultaneous uses of the sidewalk, these standards and guidelines ensure that new developments contribute to the safety, accessibility, and connectivity of their surrounding streetscape network.

The term *streetscape* can be used narrowly to mean the character of the space between building frontages and the curb (i.e., the sidewalk) or include related elements such as public open spaces, alleyways, and paseos. However, a larger and more useful definition means everything that makes up the scene on a street. Typical elements include the roadway itself, flanking buildings, sidewalks, street trees, street lights, street furniture (permanent benches, trash receptacles, etc.) in addition to adjoining public or publicly-accessible open spaces. Collectively, these elements create an outdoor environment – a place for people to circulate, gather, and be outside.

Beyond their contribution to safe and comfortable pedestrian circulation, these standards and design guidelines support the elevation of North Lake Avenue into a Great Street – a dynamic public space that fosters community, supports the local economy, and invites people to stay. To that end, this chapter begins with a special section, *Towards a Great North Lake Avenue*, which provides additional explanation of the features of a Great Street and the ways that development within the NLSP area can support that vision through design of the public realm. A lively streetscape rich with amenities will be critical to realizing the vision of the NLSP, which itself can lay the groundwork for a comprehensive and transformative streetscape plan for North Lake Avenue, as recommended in the NLSP's Vision, Goals, Policies, and Implementation Actions (Chapters 3 and 7).

The public realm standards and guidelines in this chapter address and regulate pedestrian infrastructure and amenities to support a safe, accessible, and comfortable pedestrian experience. After an introduction to the Great Streets framework, this chapter is organized into the following sections:

- » **5.1 Sidewalks**
- » **5.2 Parkways & Street Trees**

MULTI-PURPOSE STREETS

A "**Complete Street**" is a public right-of-way designed to enable safe, convenient, and accessible travel for all users, regardless of their age, ability, socioeconomic status, or mode of transport. Instead of only prioritizing drivers, they balance the needs of all users within the space constraints of the corridor. They include physical improvements that benefit active transportation (pedestrians and bicyclists) and transit, and often incorporate traffic calming measures like bike lanes, curb extensions and landscaped median islands. Where implemented, these improvements have been shown to support public health and safety, equity, and even economic development.

A "**Green Street**" is a public right-of-way designed and constructed to integrate sustainable stormwater management practices that capture, treat, and infiltrate urban runoff at its source while enhancing the pedestrian experience and promoting environmental, aesthetic, and community benefits. In the City of Pasadena, Green Streets utilize green infrastructure and nature-based elements—such as bioswales, permeable pavements, stormwater planters, and tree wells—within the parkway and curbside areas to capture and remove pollutant loads before they reach our waterways, mitigate urban heat island effects, increase urban canopy, and contribute to regional water quality goals and stormwater permit compliance. Green Streets also support the City's Climate Action Plan, Complete Streets Policy, Green Streets Policy, and long-term vision for sustainable and resilient urban infrastructure.

While one focuses on mobility and safety, and the other on environmental stewardship and resiliency, modern street construction projects can combine both principles into one design for the maximum benefit to the immediate community.

Towards a Great North Lake Avenue

GREAT STREETS are more than transportation corridors—they are dynamic public spaces that foster community, support local economies, and invite people to gather, linger, and return. They are places people want to be.

The Planning Commission Subcommittee's vision for North Lake Avenue aims to transform the corridor into a 'Great Street.' This vision prioritizes the creation of a cohesive and inviting streetscape, and the public realm standards outlined in this chapter were developed to support the Subcommittee's long-term plan for North Lake Avenue. The sidewalk and parkway requirements of the NLSP will facilitate the establishment of a high-quality public realm that's capable of accommodating various uses and users, enhancing North Lake's identity and sense of place.

Placemaking is a collaborative, community-centered approach to planning, designing, and managing public spaces that turns a space into a *place*, prioritizing people and their experiences. Instead of focusing solely on aesthetics or infrastructure, placemaking asks: How do we create spaces where people want to gather, interact, and feel a sense of belonging? Placemaking can improve not just the physical environment of a space, but the social and economic health of a community as well.

The Project for Public Spaces (PPS), a nonprofit dedicated to creating and sustaining public spaces that build strong urban fabrics through placemaking, defines the following ten qualities of a Great Street. By applying these principles, North Lake Avenue can become not only functional but also a place of community connection and everyday life.



1 Attractions & Activities

People are drawn to places that offer engaging activities—and they're more likely to come back. Without things to do, spaces stay empty and can fall into decline. Successful destinations offer a diverse range of experiences that appeal to all ages, genders, times of day and year, and group sizes. A well-planned destination links these experiences along an inviting path that encourages exploration and repeat visits.

2 Unique Image & Identity

The most memorable streets have an individual identity, shaped by their architecture, history, culture, or even the type of businesses they host. This image should feel authentic and be reinforced through signage, public art and street design, as well as upkeep, as cleanliness and maintenance are crucial to preserving a positive image and sense of pride. Local stories, materials, and design elements that reflect the community's character should be celebrated.

3 Active Street Edges

The ground level of buildings should engage the street, with features that promote interaction between indoor and outdoor spaces. Active uses like shops, cafés, or galleries make streets more vibrant and walkable than blank façades or empty lots. At a minimum, the edges should offer visual interest and transparency; blank walls, parking, and other dead zones along the sidewalk should be avoided. Uses should be designed for year-round activity and help tie both sides of the street together.

4 Essential Amenities

Well-designed amenities support a lively and functional street. These include lighting, shading, trees, landscaping, bus shelters, bike racks, waste bins, and different seating options such as single seats, benches, and café chairs. Giving people choices in where and how they sit is often overlooked but essential. Grouping amenities together encourages use and enhances comfort and convenience.

Elements of a Great Street

FURNITURE



A variety of permanent, temporary, and/or modular furniture should be widely accessible and distributed throughout, inviting people to stop and stay in the public realm.

PUBLIC ART



Murals, sculptures and other forms of art can be incorporated into all kinds of projects, including streetscape plans, development projects and infrastructure works.

SHADE



Shade trees and human-made shade structures offer protection from the sun and can contribute to pedestrian comfort in the high heat of summer.

BIKE FACILITIES



Bike racks, corrals, lockers create an environment where people feel encouraged to use alternative modes of transportation (and confident in the security of their bike).

LIGHTING



Street lamps and other forms of lighting, from string lights to building-mounted sconces to ground lighting, play a major role in the identity and safety of a street after dark.

WAYFINDING



Signage with directions and walking distances to nearby destinations and landmarks, biking and transit routes, and/or other local and historical information can help orient visitors.



5 Ongoing Management

Effective management is the backbone of a successful street. This involves more than maintenance—it includes tenant coordination and programming regular events to keep the space active. Programming can range from small performances and sales to larger civic, cultural, or seasonal events. Frequent small-scale events—like musicians, walks, or pop-up vendors can help build habit and presence.

6 Seasonal Strategies

To maintain vibrancy year-round, especially in places with less formal management, seasonal events like holiday markets, festivals, and recreational programs are vital. These strategies help draw people in during different times of the year, keeping the street animated regardless of season. Temporary elements such as seasonal installations or decor can keep the street dynamic and fresh.

7 Design for Diverse Users

A Great Street is for everyone. It should serve a mix of ages, cultures, income levels, and physical abilities. Public space feels truly vibrant when everyone feels welcome and no single group dominates. To best achieve this from the start, spaces should be designed with input from community members, including those historically excluded from planning processes, in order to ensure the design reflects their needs, values, and desires. It's about equity, representation, and flexibility.

8 Balanced Transportation

A successful street supports multiple modes of transportation, ensuring safe access for all by prioritizing walking, biking, and public transit. Use traffic calming strategies (like narrowed lanes, curb extensions, and reduced speed limits) to ensure that vehicles don't overwhelm the space. Pedestrian safety and comfort should come first; intersections and crossings must be safe spaces for people.

"The best streets are comfortable to walk along with leisure and safety. They are streets for both pedestrians and drivers. They have definition, a sense of enclosure with their buildings, distinct ends and beginnings."

— Allan B. Jacobs, author of *Great Streets*

9 Blended Spaces

Public life often happens where public and private spaces overlap. Patios, sidewalk cafés, retail displays, and shared street designs help blur this boundary, with businesses like shops and restaurants spilling onto sidewalks. Encouraging this interaction adds energy to the street and supports local businesses.

10 Neighborhood Integration

Great streets don't exist in isolation—they connect to the broader urban context of the city. Thoughtful transitions from district to district (e.g., commercial businesses to quieter residential neighborhoods) help maintain community character and reduce conflicts. Simple measures such as changes in scale, building type, street width, or landscaping can signal this shift in pace and use.

Adapted from Qualities of a Great Street, Project for Public Spaces.

5.1 Sidewalks

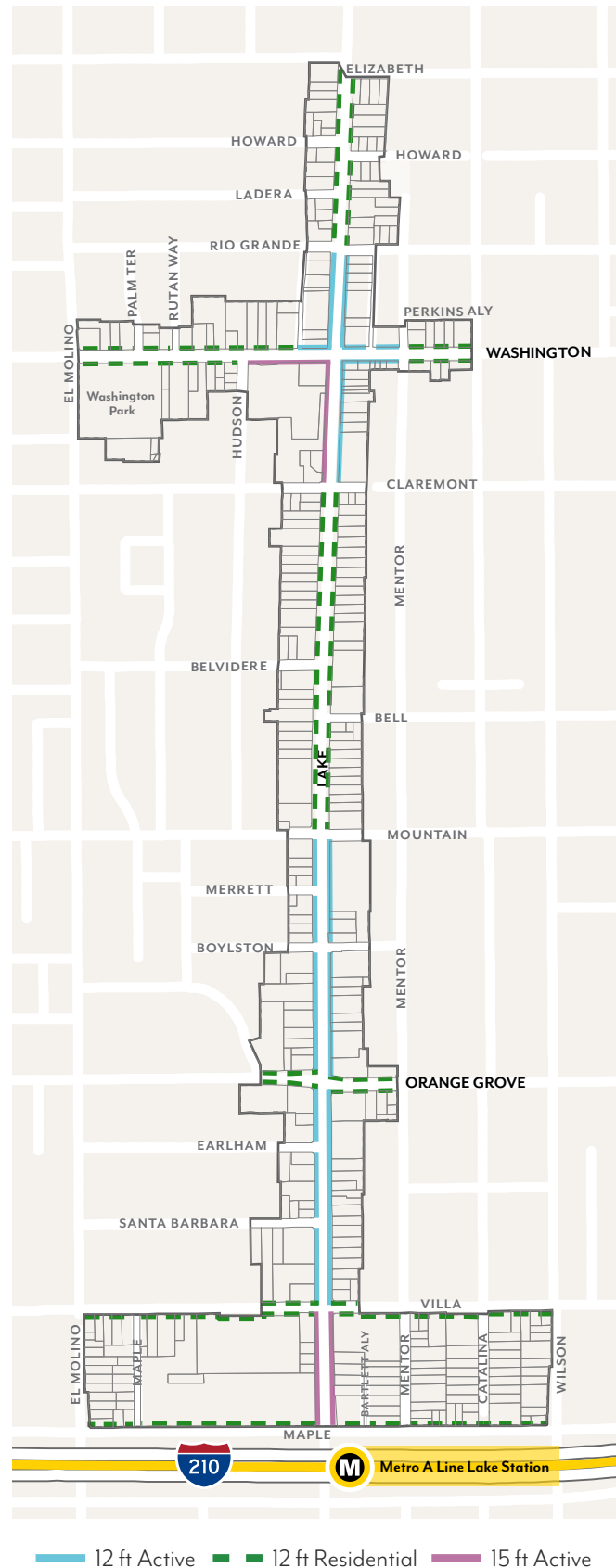
These standards are intended to:

- » Ensure a minimum sidewalk width is achieved, appropriate to support future densities, intensities, uses, and pedestrian volumes;
- » Provide sufficient space to support dedicated Amenity and Walk Zones; and
- » Increase shade, carbon sequestration, and stormwater capture by allowing adequate space for street trees and parkways.

5.1.1 SIDEWALK WIDTH

- A. **Dimension.** Projects shall provide sidewalks that meet the required widths per Map 5.1-1. Where the existing sidewalk right-of-way is less than the required width, the difference shall be provided through a private property dedication.
 1. Sidewalks are measured from the Primary Curb Line of each block to the sidewalk line, as illustrated in Figure 5.1-2.
 2. This area shall be paved for general use to the standards specified by Public Works, except for landscaped parkways per Section 5.2.
 3. Within the sidewalk width, sidewalk zones shall be provided to the dimensions set in Figure 5.1-3.
 4. Where the curb deviates (i.e. bulb-outs), exceptions in zone width are allowed and shall be determined by Public Works.
 5. Driveways are allowed per Section 6.4.2.
- B. **Maintenance.** Sidewalk improvements shall be installed and maintained by the abutting property owner.

Map 5.1-1: Sidewalk Widths



SIDEWALK WIDTHS

Sidewalk widths of at least 12' are required throughout the NLSP to provide space for a clear walk zone and basic amenities such as landscaping, lighting, signage, and bicycle parking. Sidewalks of 15' are required where feasible to increase flexibility of amenity placement and clear paths of travel.



An approximately 12' residential sidewalk.



An approximately 15' active sidewalk.

DEFINING THE SIDEWALK LINE

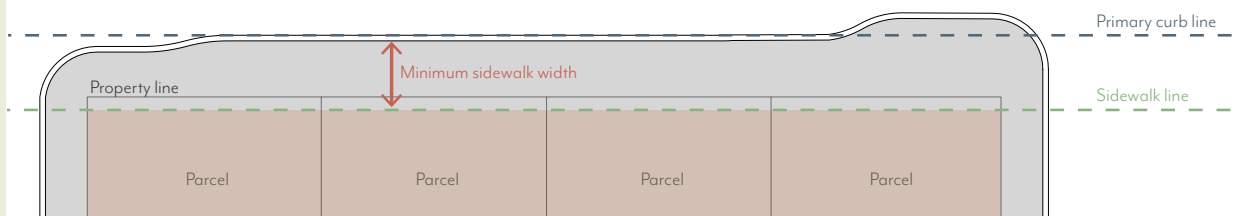
The sidewalk line is the line created by measuring the required sidewalk width (as shown in Figure 5.1-2) from the primary curb line. This curb line is the predominant face-of-curb of a given block at the discretion of Public Works, and shall not include "bulb-outs" or occasional reductions in sidewalk width at intersections.

Some parcels may not currently provide sufficient width to meet the sidewalk requirement. In these cases, the property owner must provide additional paved area through a dedication to meet the required sidewalk width.



As demonstrated by this extended "bulb-out" on North Lake Avenue, many sidewalk segments are or will be wider than the minimum width.

Figure 5.1-1: Sidewalk Width Measurement



PASADENA STREET DESIGN GUIDE

Pasadena's Street Design Guide provides a framework for understanding the way sidewalks are used, and organizes sidewalks into zones to avoid conflict between various uses and amenities.

Requirements vary based on the level of activity, land uses, intensities, and densities, as well as special conditions. Through designating specific zones, the NLSP can help enhance the pedestrian experience by increasing sidewalk widths, enabling more shade coverage and opportunities for amenities such as seating and landscaping.

The Street Design Guide organizes sidewalks into the following three zones (see Figure 5.1-2), which provide a basis for standards in the Specific Plan:

- » The **Amenity / Curb Zone (Amenity Zone)** is the portion of the sidewalk directly adjacent to the street right-of-way. This zone typically includes street trees, street lights, parkways, street furniture, bicycle parking, bus shelters, and other utility facilities.
- » The **Walk Zone** is the portion of the sidewalk dedicated to pedestrian travel and shall be free of obstructions.
- » The **Building Frontage Zone** is adjacent to private property and allows for outdoor furniture and shade structures.

Figure 5.1-2: Sidewalk Zones



5.1.2 SIDEWALK ZONES

- A. **Amenity Zone.** Sidewalks shall provide an Amenity Zone consistent with the width illustrated in Figure 5.1-3, including the curb.
- Projects shall meet minimum parkway and street tree requirements per Section 5.2.
 - The following elements are permitted in the Amenity Zone at the discretion of the Director of Public Works and with public works permits (if applicable):
 - Paved area for pedestrian mobility,
 - Parkways and street trees,
 - Seating/furniture,
 - Outdoor dining,
 - Planters,
 - Bicycle parking,
 - Bus shelters, and/or
 - Other utility facilities including streetlights, signals, meter/sign poles, pullboxes, etc.
- B. **Walk Zone.** Sidewalks shall maintain a Walk Zone as a continuous path of travel for pedestrians at the width illustrated in Figure 5.1-3. This area shall be free of all furnishings, landscaping, or obstructions.
- C. **Frontage Zone.** Sidewalks may include a Building Frontage Zone between the Walk Zone and the sidewalk line. A maximum width is illustrated in Figure 5.1-3.
- The following elements are permitted in the Frontage Zone:
 - Paved area for pedestrian mobility,
 - Seating/furniture,
 - Outdoor dining (with a Public Works permit),
 - Planters, and/or
 - Shade structures per Section 6.2.4.

Figure 5.1-3: Sidewalk Zones

Diagram 1: 12' Active Sidewalks

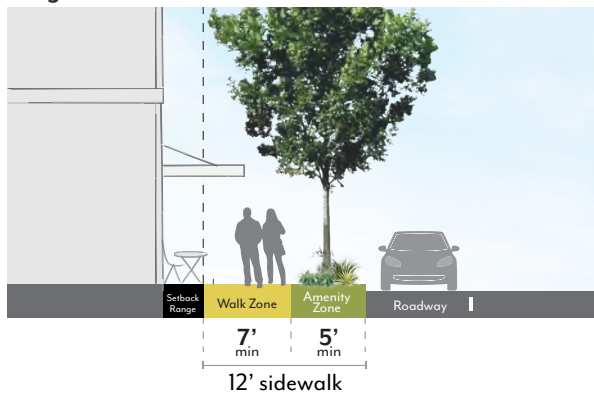
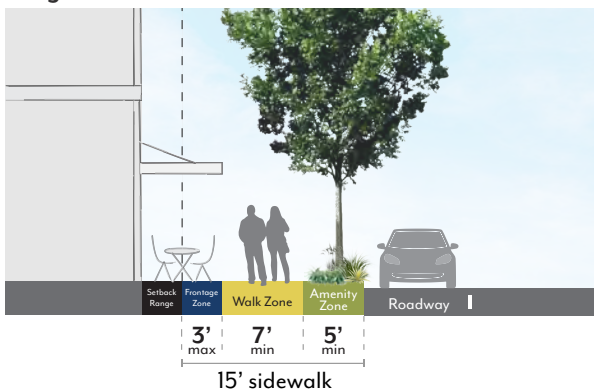


Diagram 2: 12' Residential Sidewalks



Diagram 3: 15' Active Sidewalks



TYPES OF SIDEWALKS

Active sidewalks, used in more commercial and mixed-use areas, have a wider Walk Zone to accommodate higher levels of pedestrian activity.

Residential sidewalks, used in lower-density and less commercialized areas, have wider parkways to soften the street with more landscaping and help buffer a roadway's traffic.

*Example setback conditions illustrated.
Refer to Section 6.1.4 for required setback dimensions.*

SIDEWALK ZONES & FEATURES

Walking in Pasadena should be a pleasant and comfortable experience. Long walks without places to pause and rest may become daunting for some pedestrians. Streetscape design and amenities should therefore emphasize pedestrian safety and comfort. Street furniture, when combined with street trees and proper lighting, humanize and communicate the true public nature of our downtown streets. For instance, the proper placement of street furniture introduces distance and a perceived protection from vehicular traffic. Especially in commercial areas, these furnishings should include benches and trash receptacles.

BUILDING FRONTAGE ZONE



Frontage zones may be used to accommodate tables and chairs for outdoor dining.



Frontage zones may include planters with landscaping to enhance the ground floor.

WALK ZONE



Walk zones of 5 feet allow two people to walk together comfortably.



Wider walk zones of at least 6-7 feet are appropriate for commercial retail areas.

AMENITY/CURB ZONE



Grass or turf parkways with shade trees are appropriate for residential areas.



Amenity zones may include curb extensions which increase the sidewalk width while also contributing to traffic calming.

5.2 Parkways & Street Trees

These standards are intended to:

- » Enhance pedestrian conditions through increased landscaping at sidewalk level;
- » Provide a visual buffer between parking lane and sidewalk;
- » Improve stormwater capture and increase permeability of sidewalk zone; and
- » Improve street tree health and support the process of carbon sequestration.

5.2.1 PARKWAYS

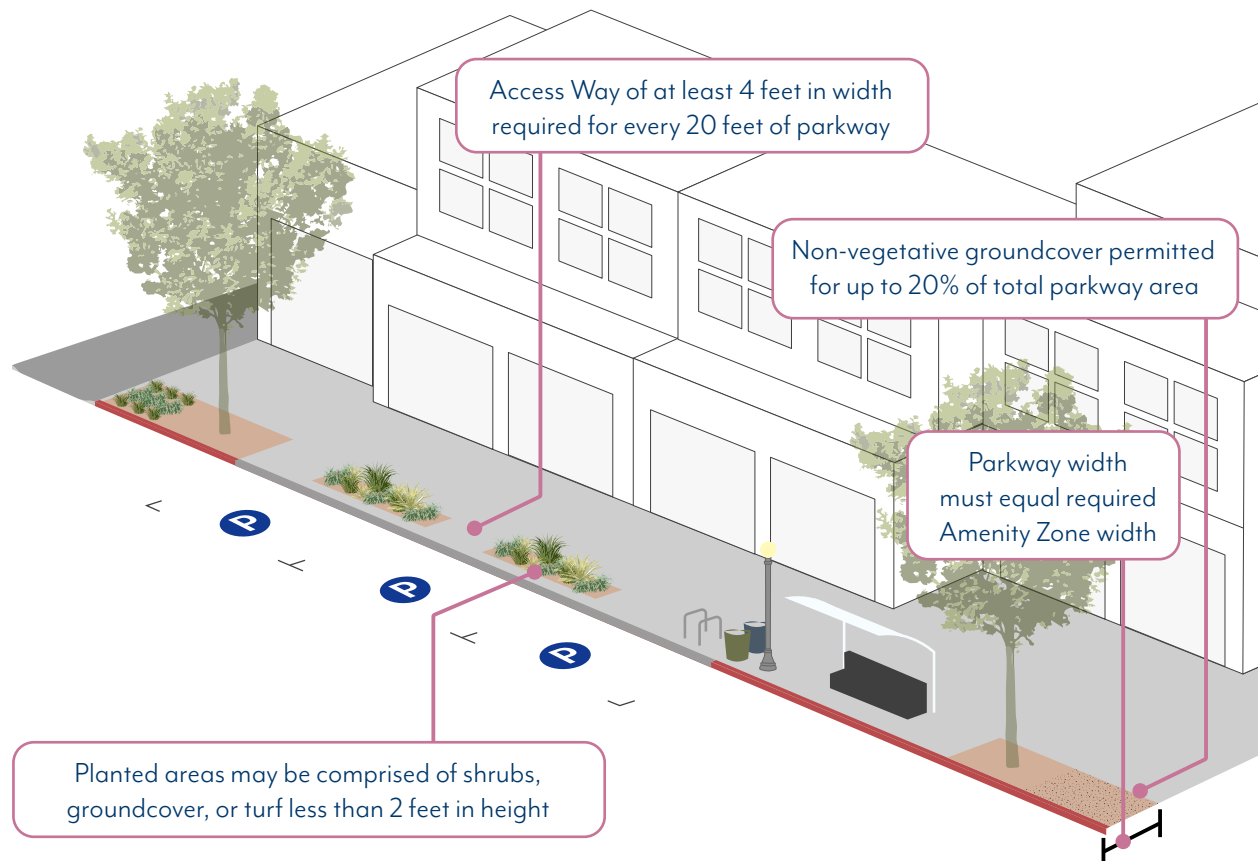
- A. **Required Parkway.** Projects shall include parkways within the Amenity Zone as follows; see Figure 5.2-1.
1. In all other zoning districts, parkway length shall be no less than 30 percent of street frontage, unless approved by the Director of Public Works.

THE IMPORTANCE OF PARKWAYS

Parkways are landscaped or permeable areas within the sidewalk that play an important role in the urban landscape by improving pedestrian comfort, increasing sustainability, and enhancing the aesthetic character of the public realm. By expanding the permeable area around street trees, parkways increase rain and stormwater capture, leading to improved street tree health and larger tree canopies, which creates cooler temperatures for pedestrians, helps to sequester carbon from the atmosphere, and reduces pollution in our nearby waterways.

Parkways also provide a visual buffer between the pedestrian and moving or parked vehicles, which further improves pedestrian comfort and creates a more attractive sidewalk environment. Typically residential neighborhoods can accommodate long, uninterrupted areas of parkways within the sidewalk. In commercial and mixed-use areas, available space for parkways may be constrained by bus shelters, street lights, and the need to accommodate higher levels of pedestrian traffic.

Figure 5.2-1: Parkway Design Standards



PARKWAYS & STREET TREES

Parkways and street trees provide numerous benefits and are an indispensable part of the NLSP environment. Most obviously, landscaping enhances the visual quality of the area. But parkways bring sustainable benefits such as stormwater capture, while street trees provide cooling effects and contribute to the spatial definition of the street that creates a human-scaled space with a comfortable sense of enclosure. Trees also provide overhead cover to pedestrians, offering shade and reasonable protection from the sun and rain. In general, parkways and street trees add a gracious quality to Pasadena.



Existing parkways should be maintained or expanded with the use of California native plants, softening the public realm and providing opportunities for stormwater capture.



Consistent tree size and spacing along a sidewalk creates a pleasant rhythm and reinforces the space of the street.

LANDSCAPED MEDIANS

Street medians may be used to incorporate landscaping and additional trees, providing many of the same benefits as landscaped parkways, while enhancing the street's overall shade canopy, visual character, and safety. Flowering accent trees in street medians complement street tree recommendations and add to the streetscape's distinct identity and seasonal variation.



Landscaped medians add a variety of benefits including visual interest, cooling, stormwater capture, and traffic calming.



While trees in the parkway should provide a pedestrian shade canopy, street medians are more appropriate for planting decorative trees that provide visual accent and add to the streetscape's distinct identity.

- B. **Dimensions.** Parkway shall be constructed at the same width as the Amenity Zones illustrated in Figure 5.1-3, minus the 6-inch width required for the curb.
1. When street parking is adjacent to the curb, a paved buffer with a minimum width of 18 inches is required, in addition to the 6-inch curb, except where tree grates are adjacent to the curb.
 2. Barriers up to 24 inches high, such as low walls or fences, are permitted at the interior edge of the parkway but are not required.
- C. **Access Ways.** Where on-street parking is permitted, access ways shall be provided at a minimum frequency of one per every 20 feet of continuous parkway.
1. Access ways shall be a minimum of 4 feet in width and provide a firm, uniform walking surface in all weather conditions from the curb to the Walk Zone.
 2. The finished surface of access ways shall be in plane with both the adjoining top of curb and sidewalk.
 3. Access ways shall be constructed of pavers, concrete, or stabilized decomposed granite.



Parkway with street trees and low perennial plantings

- D. **Planted Area.** A minimum of 80 percent of the total required parkway area for a given project shall be comprised of plant material.
1. Permitted materials include groundcovers, turf or turf substitutes, and shrubs or low perennials that are lower than 24 inches in height at full maturity.
 - a. All plant material shall be native or climate appropriate and have a water use rating of Moderate, Low or Very-Low as defined by Water Use Classification of Landscape Species (WUCOLS) for the region. Plant water use requirements may be relaxed to maximize the efficiency of parkway stormwater capture systems subject to approval by the Director of Public Works.
 - b. Plants with spines or thorns shall not be planted adjacent to any walkways or curbs.
 - c. Edible plants are not permitted in parkways.
 - d. Artificial turf is not permitted in parkways.
 2. When removing existing plant material like turf grass from a parkway, there shall be no damage to the street tree roots. Parkway improvements involving excavation within an existing tree's root zone must be consistent with the City's Tree Protection Guidelines. Root pruning, if required and approved by Public Works, must be overseen by a Certified Arborist. Excavation within a tree's root zone must be replanted immediately to prevent the tree roots from exposure and undue harm.

MATERIALS & ACCESS GUIDELINES

- » Native plants and those which require little or no irrigation are preferred.
- » Plants are not recommended to be planted within 4 feet of a tree trunk.
- » In areas with high pedestrian traffic, plant material should have a minimum height of 18 inches to discourage pedestrians from stepping on the parkway. Groundcover is discouraged unless it can withstand heavy foot traffic.

- E. **Non-planted Area.** A maximum of 20 percent of the parkway area may be organic or inorganic cover.
1. Permitted materials include permeable pavers, decomposed granite, gravel, rocks, or mulch.
 - a. Pavers are not allowed within 3 feet of any public streetlight pole or pull box or other utility facilities.
- F. **Stormwater Management.** Parkway shall either meet the following basic stormwater standards, or propose a biofiltration planter or swale design based on local conditions subject to approval of the Director of Public Works.
1. The parkway shall be at the same grade as the adjacent hardscape surface at the outer edge of the parkway and slope at a minimum of 1 percent towards the center of the parkway.
 2. For parkways with a width greater than 5 feet, the center 2 feet of the parkway shall be depressed 3 to 4 inches to form a shallow swale to collect sidewalk stormwater. Alternative means of storing runoff, such as gravel sumps within the parkway, may be provided with review authority approval.
- G. **Irrigation.** Irrigation systems in parkways shall be designed and constructed in a manner that will eliminate surface runoff onto any impermeable surface, public or private, under any condition.
1. Design of irrigation systems in parkways shall be in accordance with all local, state, and federal laws and regulations for water conservation.
 2. Street tree roots shall not be damaged during the irrigation installation process.
- H. **Maintenance.** Abutting property owner shall maintain the parkway in a condition so as not to endanger persons or property, and not to interfere with the public convenience.

STORMWATER & IRRIGATION GUIDELINES

- » Parkway should be designed to treat and/or capture stormwater run-off from the adjacent to the greatest extent feasible given soil conditions.
- » Suspended pavement systems are encouraged as a means of controlling runoff volume and should be implemented under and adjacent to large pedestrian walkways.
- » If impermeable surfaces are used within parkways, they shall be constructed to drain to permeable areas.
- » Low-volume, sub-surface/drip irrigation or other non-spray irrigation systems or hand-watering is preferred where irrigation is needed.



Mid-block curb extensions can be used for street trees and stormwater retention.

5.2.2 STREET TREES

- A. **Species.**¹ Street tree species shall be selected according to the Master Street Tree Plan at the discretion of the Director of Public Works. Trees may be planted within parkways or tree wells.
- B. **Spacing.** Street trees shall be planted at a spacing of one per 30 feet. Exceptions can be made by the Director of Public Works due to conflicts with street lights, bus shelters, utility boxes, other street amenities or species type. Closer spacing is encouraged where feasible/when appropriate for a particular tree type.
- C. **Well Dimension.** Tree well width shall be equivalent to the required Amenity Zone, minus the 6-inch width required for the curb.
 1. If a paved buffer zone is required due to adjacent street parking, the tree well width may be reduced to accommodate this buffer strip. The minimum length of a tree well shall be 6 feet.
 2. Street trees planted within tree wells must be installed according to the Department of Public Works Tree Planting in Tree Well Standard Plan.
- D. **Well Frames.** Tree well frames, or tree grates, may be installed according to the Department of Public Works Tree Well Frame Installation Standard.
- E. **Expanded Root Zone Cell.** Each street tree shall be provided with an uncompacted root zone volume of 800 cubic feet minimum. The root zone volume depth shall be 2 feet minimum and 3 feet maximum.
 1. Where this root zone volume cannot be provided within the parkway area, an expanded root zone cell volume shall be provided below adjacent pavement using a strategy such as structural soil or a suspended pavement system to provide an uncompacted soil area suitable for tree root growth.
 2. The root zone volume per tree requirement may be reduced by 10 percent where two or more trees share a contiguous root zone cell.
- F. **Maintenance.** All street trees shall be maintained by the Department of Public Works.

¹ See **Appendix A.2 Design Guidance for Tree Selection** for detailed recommendations to better align North Lake Avenue's street tree species with the vision, goals, and policies in this Specific Plan related to shade, climate resilience, stormwater capture, and supporting a vibrant public realm.

THE IMPORTANCE OF STREET TREES

Street trees play an important role in keeping cities livable, sustainable and resilient. Trees improve air quality, increase urban biodiversity, and help reduce carbon emissions. In addition to environmental benefits, trees provide health, social, economic, and aesthetic benefits to communities. Requirements based on guidance from the City's Master Street Tree Plan will increase street tree coverage and require the preservation or introduction of certain tree species. In adherence with these street tree standards and guidelines, new development will contribute to an enhanced shade canopy that helps to reduce the urban heat island effect, decrease sidewalk temperatures, enhance pedestrian comfort, and improve the visual experience of the street.



A street tree with healthy canopy and sufficient tree well size.

Ch. 6

Private Realm Standards

Private Realm Standards	87
6.1 Scale	90
6.2 Frontage	100
6.3 Open Space	109
6.4 Parking	114



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Private Realm Standards

CHAPTER OVERVIEW

The private realm objective development and design standards in this chapter serve to implement the vision, goals, and policies of the NLSP, described in Chapter 3.

In addition to the requirements of the NLSP, all projects shall comply with the Pasadena Municipal Code (PMC) requirements below. In the event of conflict between the Zoning Code and the NLSP, the requirements of the specific plan shall control (PMC 17.12.020.D).

- » PMC 17.40 General Development
- » PMC 17.42 Inclusionary Housing
- » PMC 17.43 Density Bonus
- » PMC 17.44 Landscaping
- » PMC 17.46 Parking & Loading
- » PMC 17.48 Signs
- » PMC 17.50 Specific Land Uses

Per Section 4.1, development standards for the NL-RM-48/32/16, NL-RS-6, OS and PS zoning districts are not included in the NLSP. In NL-RM-48/32/16 and NL-RS-6, development shall be subject to the standards of the corresponding zoning in PMC 17.22. In OS and PS, development shall be subject to a Conditional Use Permit or Master Plan per PMC 17.26.

Guidelines, incorporated as part of this plan in shaded text boxes, are intended to encourage quality architecture that enhances the community's unique character. Projects should also consult Pasadena's *Design Guidelines for Neighborhood Commercial and Multi-Family Districts* for further guidance on building form and relationship to the surrounding neighborhood. Projects required to go through Design Review will be assessed based on the project scope and the standards and guidelines of the NLSP, as well as the *Design Guidelines for Neighborhood Commercial and Multi-Family Districts*.

This chapter is organized into the following sections:

» 6.1 Scale

- » 6.1.1 Density (du/ac)
- » 6.1.2 Intensity (FAR)
- » 6.1.3 Height
- » 6.1.4 Setbacks
- » 6.1.5 Stepbacks
- » 6.1.6 Historic Adjacency
- » 6.1.7 Modulation

» 6.2 Frontage

- » 6.2.1 Ground Floor Frontages
- » 6.2.2 Ground Floor Design
- » 6.2.3 Transparency
- » 6.2.4 Shade Structures
- » 6.2.5 Arcades & Galleries
- » 6.2.6 Walls & Fences
- » 6.2.7 Balconies & Roof Decks

» 6.3 Open Space

- » 6.3.1 Minimum Area
- » 6.3.2 Private Open Space
- » 6.3.3 Common Open Space
- » 6.3.4 Publicly Accessible Open Space
- » 6.3.5 Paseos

» 6.4 Parking

- » 6.4.1 Minimum Parking
- » 6.4.2 Vehicle Access
- » 6.4.3 Layout & Design

Table 6-1: Summary of Development & Design Standards

Standard	NL-MU-C	NL-MU-N
Scale		
Allowable Density	Section 6.1.1	
Dwelling Units per Acre	Map 6.1-1	
Allowable Intensity	Section 6.1.2	
Floor Area Ratio	Map 6.1-2	
Building Height	Section 6.1.3	
Height	Map 6.1-3	
Required Setbacks	Section 6.1.4	
All streets	Map 6.1-4	
Adjacent to RM/RS	15' min.	
Other interiors	None required	
Required Stepbacks	Section 6.1.5	
Adjacent to RM/RS	Figure 6.1-2	
Historic Adjacency	Section 6.1.6	
Setbacks & Stepbacks	Figure 6.1-3	
Modulation	Section 6.1.7	
Length	10% or 20' break required for buildings exceeding 150' street frontage	
Area	25% for buildings over 50' in length	
Frontage		
Ground Floor Frontages	Section 6.2.1	
Required Uses	Table 6.2-1 and Map 6.2-1	
Commercial Depth	35' average, 20' min.	
Ground Floor Design	Section 6.2.2	
Height	15' min.	
Residential Elevation	-2' to 6' max.	
Minimum Transparency	Section 6.2.3	
Ground Floor	60%	50%
Overall Façade	30%	30%
Residential Units	15%	15%
Shade Structures	Section 6.2.4	
Arcades & Galleries	Section 6.2.5	
Walls & Fences	Section 6.2.6	
Balconies & Roof Decks	Section 6.2.7	

Standard	NL-MU-C	NL-MU-N
Open Space		
Minimum Open Space	Section 6.3.1	
Non-residential	5% of Non-residential Gross Floor Area for projects over 40,000 sf	
Residential	125 sf per studio, 150 sf per 1-bed, 200 sf per 2-bed, 250 sf per 3+bed	
Publicly Accessible	2% of GFA for projects over 80,000 sf; none required otherwise	
Private Open Space	Section 6.3.2	
Common Open Space	Section 6.3.3	
Publicly Accessible Open Space	Section 6.3.4	
Paseos	Section 6.3.5	
Parking		
Minimum Parking	Section 6.4.1	
Vehicle Access	Section 6.4.2	
Layout & Design	Section 6.4.3	
Other Applicable Standards ¹		
General Development	PMC 17.40	
Inclusionary Housing	PMC 17.42	
Density Bonus	PMC 17.43	
Landscaping	PMC 17.44	
Parking & Loading	PMC 17.46	
Signs	PMC 17.48	
Specific Land Uses	PMC 17.50	

¹ Projects shall follow all requirements listed except where modified by this Specific Plan. In the event of conflict between the Zoning Code and this Specific Plan, the requirements of this Specific Plan shall control, per PMC 17.12.020.D.

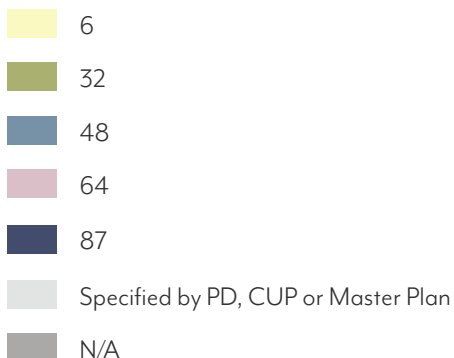
6.1 Scale

These standards are intended to:

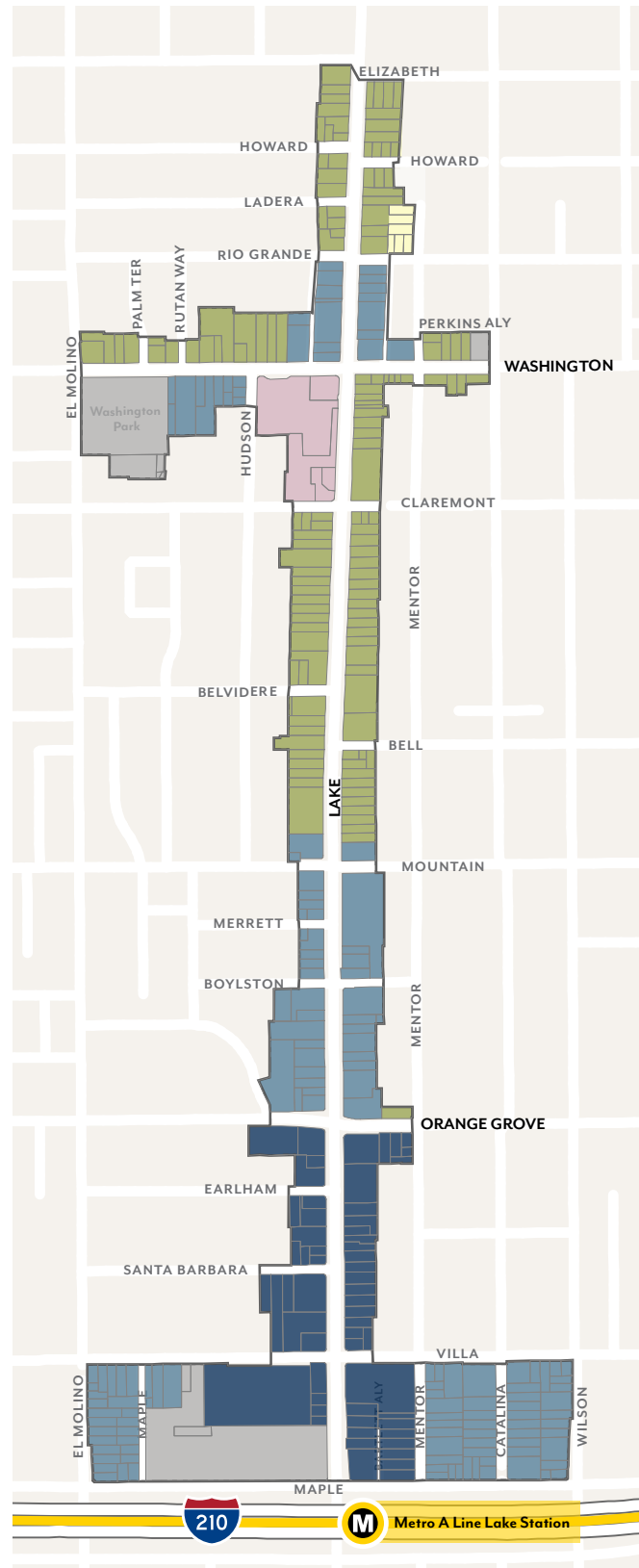
- » Implement the General Plan density (du/ac) and floor area ratio (FAR) values;
- » Shape development in a manner that creates a defined public realm and appropriate scale of buildings for a visually appealing community;
- » Reduce building massing through setback and stepback requirements that create appropriate transitions to residential neighborhoods;
- » Support high-quality architecture and urban design through modulation requirements and a varied roof lines incentive; and
- » Require appropriate transitions to designated historic resources.

6.1.1 DENSITY

- A. **Residential Density.** Projects that include residential dwelling units shall not exceed the allowable dwelling units per acre (du/ac) set in Map 6.1-1.
1. Fractions shall be rounded to the nearest whole number; those at 0.50 may be rounded up.
 2. For projects utilizing state density bonus, refer to Government Code 65915.
 3. The maximum is based on site area. If a street dedication or easement is required, density shall be calculated using the size of the lot prior to the street dedication or easement.

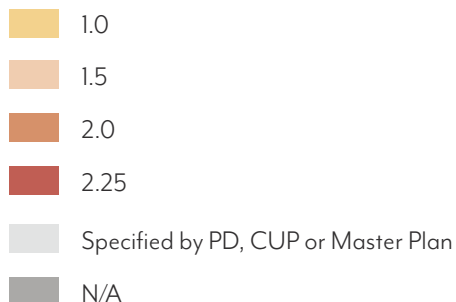


Map 6.1-1: Residential Density

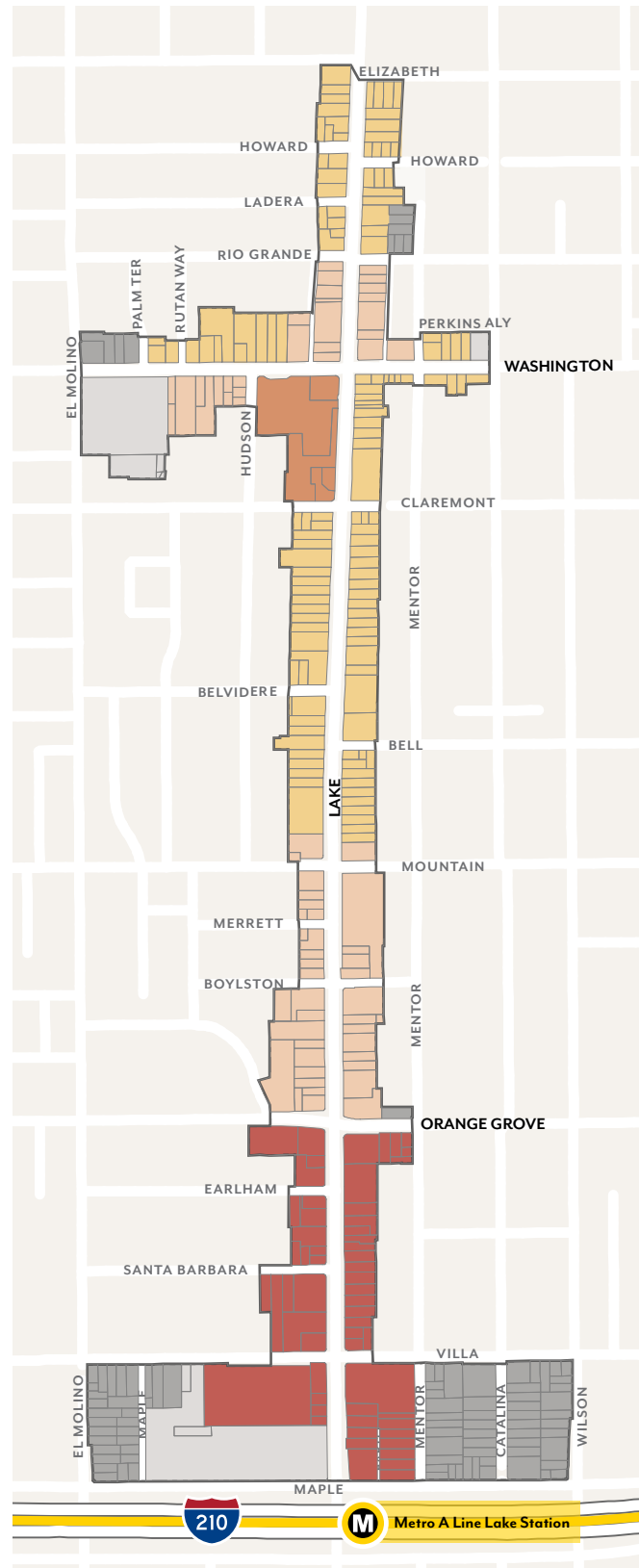


6.1.2 INTENSITY

- A. **Floor Area Ratio.** Projects that include non-residential space, including mixed-use, shall not exceed the allowable floor area ratio (FAR) set in Map 6.1-2.
1. In mixed-use projects, residential floor area is included in FAR.
 2. Areas used exclusively for vehicle and bicycle parking and loading are excluded from FAR.
 3. The maximum is based on site area. If a street dedication or easement is required, FAR shall be calculated using the size of the lot prior to the street dedication or easement.



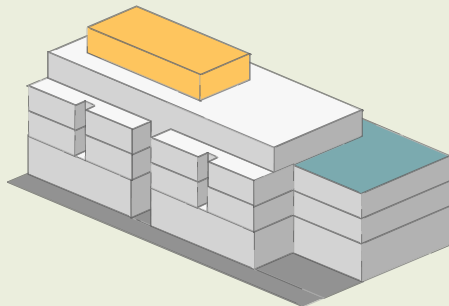
Map 6.1-2: Floor Area Ratio



6.1.3 HEIGHT

- A. **Building Height.** Projects shall not exceed the height limits set in Map 6.1-3.
1. Height is measured per PMC 17.40.060.
 2. Exceptions allowed for Height Averaging (6.1.3.B) and projecting features such as appurtenances and railings per PMC 17.40.060.
- B. **Height Averaging.** With approval of Design Commission, height limits may be exceeded for up to 30% of the building footprint to the maximum set in Map 6.1-3, provided that the average height over the entire footprint does not exceed the allowable height; see Figure 6.1-1.
1. The intent is to counterbalance additional height with lower heights elsewhere on-site to achieve an economically-feasible development that protects view corridors and contributes to a more visually-compelling skyline.
 2. Averaging is not applicable to other development standards relating to building scale such as setbacks. It may not be used in combination with a concession for building height when utilizing PMC 17.43.

Figure 6.1-1: Height Averaging



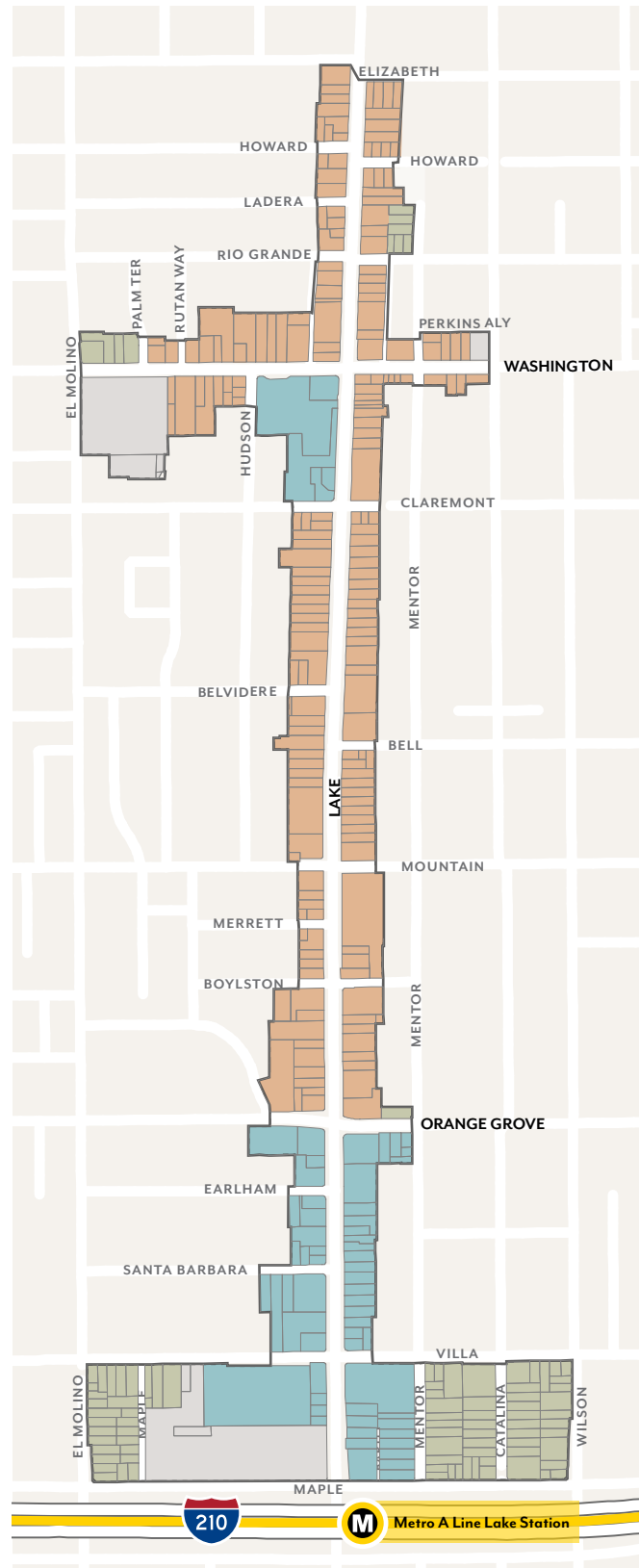
A building may exceed its height limit for up to 30% of its footprint if another area is lowered so that the average height is at or below the height limit.

Note: Diagrams used for illustrative purposes only.

Maximum Height (Height Averaging)

39 ft	Refer to PMC
51 ft (63 ft)	Specified by PD, CUP or MP

Map 6.1-3: Building Height



BUILDING HEIGHTS

The images below reflect a range of building heights that are allowed in zoning districts throughout the NLSP area. Architectural features such as roof shape, stepbacks, and modulation can add variation to the roof line and reduce visual impact of building height. *These examples are illustrative and may not reflect all applicable development standards.*



An approximately 35', 3-story building.



An approximately 39', 3-story building.



An approximately 51', 4-story building.

C. SETBACKS

- D. **Street Setbacks.** Buildings shall comply with the street setbacks set in Map 6.1-4. Setback ranges establish a minimum and maximum for the specified percentage of linear street frontage.
1. Street setbacks are measured from the sidewalk line; see Figure 5.1-2.
 2. Minimum setbacks shall apply to all stories of a building; setbacks less than the minimum are prohibited. Maximum setbacks shall apply only to the ground floor.
 3. Residential units on the ground floor shall have a minimum setback of 5 feet. Where ground floor residential units are elevated between 4 and 6 feet above sidewalk elevation, a minimum setback of 8 feet shall be required.
 4. Exceptions allowed per PMC 17.40.160 (Table 4-1).
 5. For buildings set back from the sidewalk line 5 feet or more, a minimum percentage of the setback area shall be landscaped with trees, shrubs, and/or groundcover, either in the form of in-ground landscaping or planters; see Table 6.1-2.
 6. Features allowed within the street setback include:
 - a. Landscaping and planters per PMC 17.44;
 - b. Hardscape (e.g. stoops, patios);
 - c. Shade structures per Section 6.2.4;
 - d. Arcades and galleries per Section 6.2.5;
 - e. Walls and fences per Section 6.2.6;
 - f. Seating and furniture;
 - g. Outdoor dining; and
 - h. Other open space amenities per review authority approval.

- 3-5' for 75%
- 5-10' for 50%
- 10-20' for 50%
- Specified by PMC

Map 6.1-4: Street Setbacks

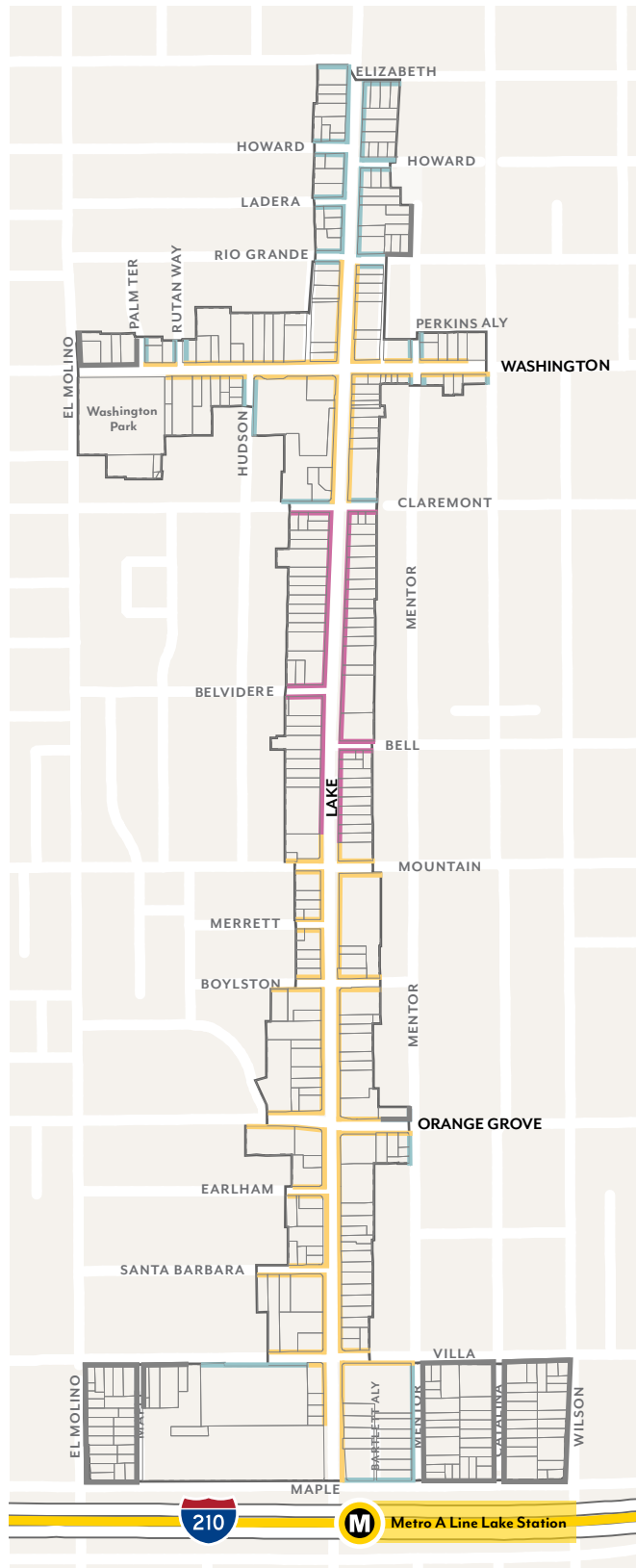


Table 6.1-2: Minimum Landscape Setbacks

Frontages with shared entrances to internal circulation	50%
Frontages with individual residential unit entrances	30%
with a stoop taller than 30 inches	10%
Frontages with individual commercial tenant entrances	30%
with outdoor dining	10%

- E. **Interior Setbacks.** Projects shall have a minimum setback based on the shared property line, whichever is greater; see Table 6.1-3. No setback is required along other interior property lines, except where modified for historic adjacency per Section 6.1.6.
1. Interior setbacks are those abutting other parcels along non-street side and rear property lines and are measured from the shared property line.
 2. Exceptions allowed per PMC17.40.160 (Table 4-1).

SETBACKS TO SUPPORT A HEALTHY TREE CANOPY

Employing an increased street setback within the established range can support street trees by creating additional space for tree canopies to grow. Street setbacks can encourage sensitive building design to accommodate both existing and new street trees, leading to healthy tree growth, additional shade, and greenery.



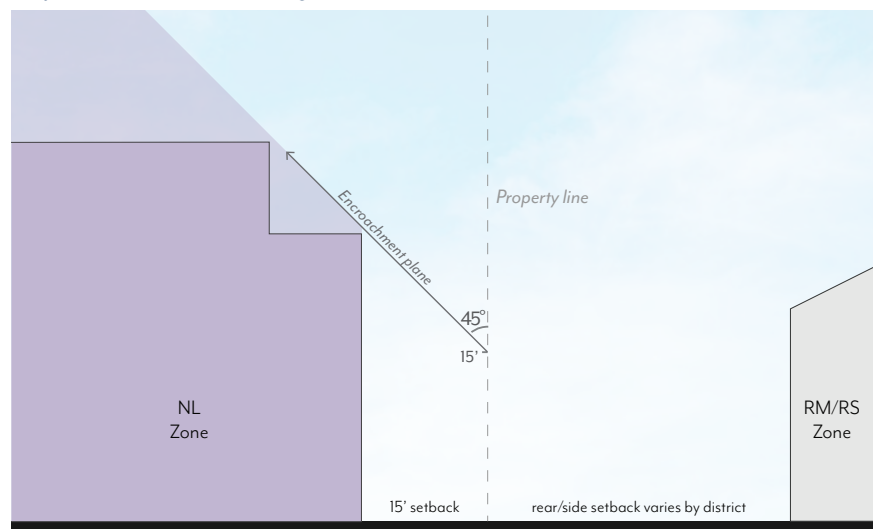
6.1.4 STEPBACKS

- A. **Interior Stepbacks.** Adjacent to RM/RS zoning districts, buildings shall not be located within the encroachment plane sloping upward and inward at a 45-degree angle measured from the vertical, commencing 15 feet above the existing grade along the shared property line; see Figure 6.1-2.
1. Exceptions allowed per PMC17.40.160 (Table 4-2.1).

Table 6.1-3: Minimum Interior Setbacks

Adjacent to RM/RS zoning districts	15 ft
Within Vineyard Gardens subarea	5 ft

Figure 6.1-2: Interior Setbacks & Stepbacks Adjacent to RM/RS Zoning Districts



6.1.5 HISTORIC ADJACENCY

- A. **Landmark Properties.** Projects on parcels with a historic resource shall be subject to review for consistency with the Secretary of the Interior's Standards.
- B. **Transition Massing.** Projects sharing a property line with a designated historic resource shall be subject to the following modified standards; see Figure 6.1-3.
 1. **Street Setbacks:** The minimum street setback shall be an average of the minimum setback in Map 6.1-4 and that of the resource for a minimum of 20 feet from the shared property line. If between two resources, the street setback shall be an average of the setbacks of the two resources.
 2. **Interior Setbacks:** The minimum interior setback shall be equal to that of the historic resource or 15 feet, whichever is less. No setback is required where the resource is built to the shared property line.
 3. **Streetwall Height:** A maximum streetwall height shall not exceed the height of the historic resource for a minimum of 20 feet from the shared property line. A stepback with a minimum depth of 10 feet is required above this height, measured from the modified minimum street setback.
 4. **Interior Stepbacks:** Projects shall not be located within an encroachment plane sloping upward and inward at a 30-degree angle measured from the vertical, commencing 15 feet above the existing grade at the property line. This plane is not applicable if the resource is built to the shared property line.

HONORING HISTORIC SIGNIFICANCE

In addition to preserving historic landmarks throughout the district, modified standards for adjacent properties ensure that historic structures do not appear diminished or incongruous with new developments in the surrounding area.

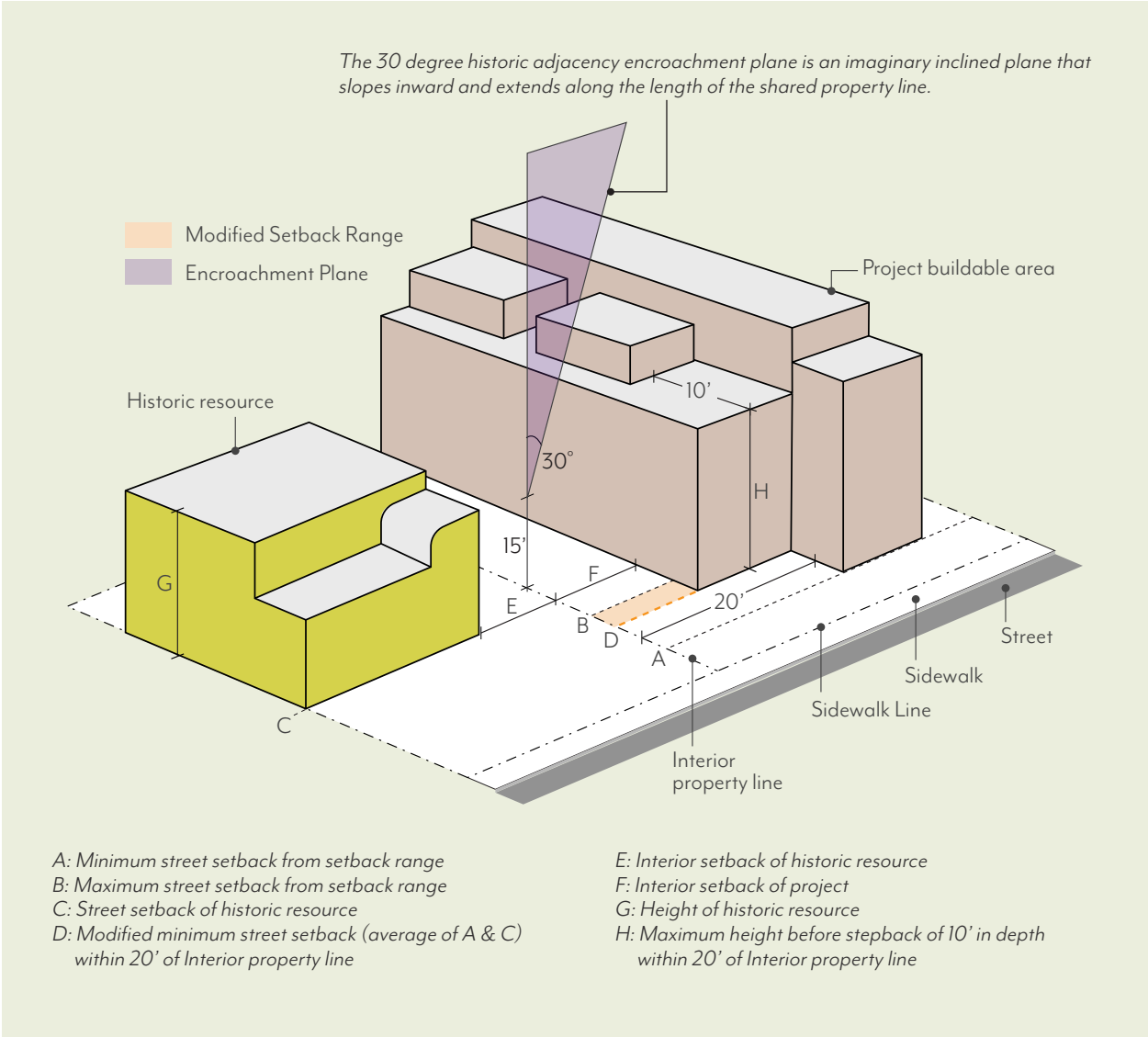


Oversen Building

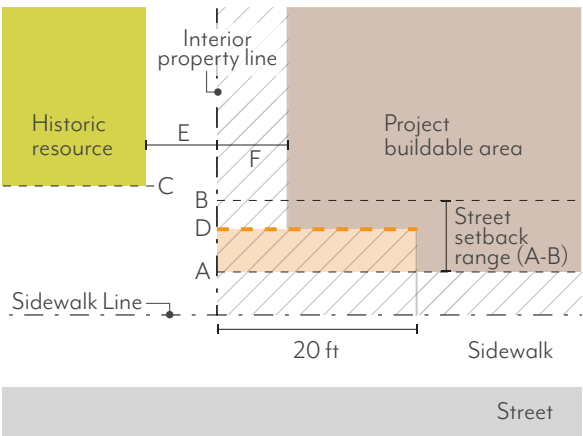


Washington Theater Building

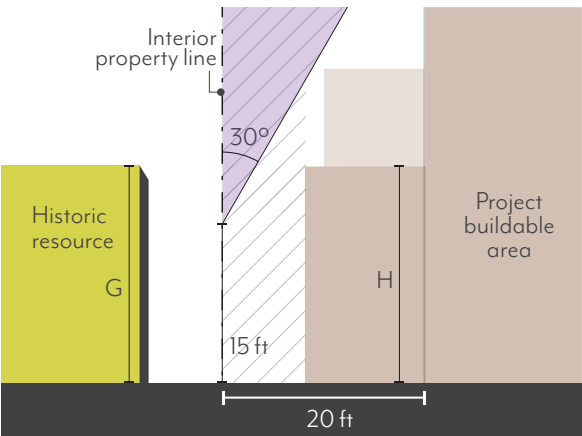
Figure 6.1-3: Historic Adjacency Transition Massing



PLAN VIEW



ELEVATION VIEW

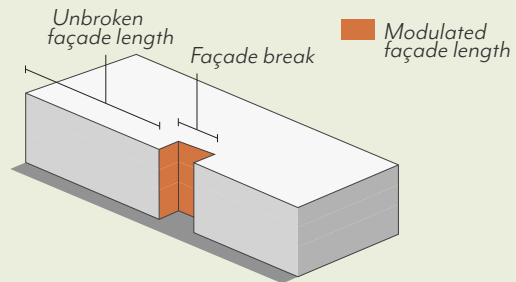


Note: Diagrams used for illustrative purposes only.

6.1.6 MODULATION

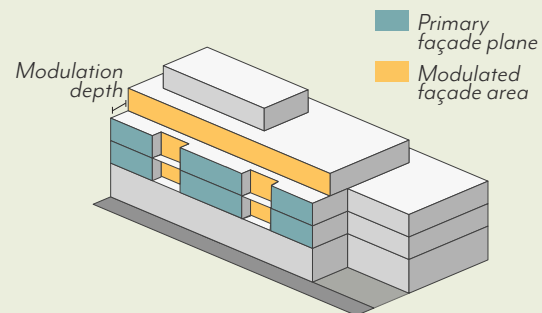
- A. **Façade Length.** Each street-facing façade exceeding 150 feet in length shall include a minimum break of 10 percent of the façade length or 20 feet, whichever is greater, such that no unbroken façade is longer than 150 feet.
1. This façade break shall be a minimum of 10 feet deep, open to the sky; see Figure 6.1-4.
- B. **Façade Area.** Street-facing façades exceeding 50 feet in length shall modulate a minimum of 25 percent of the area above the ground floor between 2 feet and 12 feet in depth from the primary façade plane; see Figure 6.1-5. Buildings with a total of 2 stories or less are exempt.
1. The primary façade plane is defined as the vertical plane with the greatest surface area above the ground floor.
 2. Planes that are separated by a gap of at least 20 feet in width and 20 feet in depth shall be considered separate façades for the purposes of this standard.
 3. Modulation is not required to be continuous or open to the sky, and may be recessed or projected, but not past the sidewalk line.
 4. Required stepbacks (6.1.5.A), required façade breaks (6.1.7.A), and projected balconies (6.2.8.A) shall not count toward the modulation requirement; balconies that are recessed a minimum of 2 feet shall qualify.
 5. Exception to Façade Area requirement permitted if the street-facing façade meets all of the following standards:
 - a. The maximum façade length shall be 100 feet.
 - b. All windows shall be recessed at least 4 inches.
 - c. The façade shall use a textural material comprised of brick, stone, precast concrete, Venetian plaster, hand-troweled stucco, or porcelain tiles at least 12 by 4 inches. Ceramic tiles, porcelain tiles less than 12 by 4 inches, standard stucco, or flat, polished stone shall not qualify.

Figure 6.1-4: Modulated Façade Length



Façades over 150 feet in length shall include a break at least 10% of the façade length or 20 feet wide (whichever is greater), 10 feet deep, open to the sky.

Figure 6.1-5: Modulated Façade Area



Façades shall modulate a minimum of 25% of the area above the ground floor 2 to 12 feet in depth from the primary façade plane.

Note: Diagrams used for illustrative purposes only.

- d. The façade shall employ a tripartite division in which the façade is organized into three horizontal sections with a base, middle, and top.
 - i. The middle section shall be differentiated from both the base and the top through a change in façade plane of at least 4 inches, or a consistent horizontal band that projects at least 4 inches from the façade.
 - ii. The top section shall use a cornice that is at least 1 foot in depth.

CREATING VISUAL INTEREST AND BREAKING UP MASS

Façade modulation refers to variations in depth of a building's façade. Modulation can be achieved through architectural elements such as fenestration patterns and window bays, stepbacks, balconies, and full façade breaks. Modulation breaks up building massing, creates visual interest, and provides opportunities for open space.



Balconies and other variations in wall plane depth create visual interest.

C. **Alternative Compliance.**

1. Eligibility. Modulation standards may be reduced or otherwise modified through the Design Review process if:
 - a. A minimum of 90 percent of the provided parking is fully or partially subterranean;
 - b. A minimum of one publicly accessible open space is provided at the ground level, visible and accessible from the sidewalk;
 - c. No other concessions, waivers, or incentives have been requested, including those associated with PMC 17.43 (Density Bonus), unless the project is designed to achieve LEED Gold certification; and
 - d. The review authority makes all of the following findings.

2. Required Findings.

- a. The building design provides modulation on each street-facing façade in a manner consistent with the project's architectural style and/or immediate context, including adjacent historic resources.
- b. The building design does not cause an adverse impact on the quality of the ground floor and public realm.
- c. The ground level open space is of adequate size and integrated with the building in a functional way that ensures the space will be actively utilized.
- d. The modification will not be detrimental to the health, safety, and welfare of the public.
- e. The building design is consistent with the objectives and policies of the General Plan and NLSP, as well as all other standards of the NLSP.

6.2 Frontage

These standards are intended to:

- » Promote an active, accessible, and attractive pedestrian environment at the ground level;
- » Activate the pedestrian street experience through design and use standards;
- » Enable flexibility and adaptability over time through quality design; and
- » Support a livable urban setting comprised of a range of uses in a comfortable pedestrian environment.

GROUND FLOOR FRONTAGES

A vibrant street-level atmosphere is created through pedestrian-oriented ground floors and well-designed frontages. Creating a comfortable and inviting pedestrian environment is essential to promote other means of transportation such as walking. This experience is directly influenced by design treatments and ground floor uses working together to create a visually-engaging sidewalk environment. To accomplish this, ground floor use standards are established for each block that are complementary to the land use permissions in Chapter 4, Section 4.3. Successful ground floor design creates an inviting, visually engaging, shaded sidewalk and pedestrian environment that supports the intended commercial, residential, or mixed use character of each district.



Ground floor frontage with commercial use, shade awnings, and a transparent entrance.



Ground floor commercial use with outdoor dining.



Ground floor frontage with combination of commercial and common space for occupants of a mixed-use building.



Residential units on the ground floor buffered by landscaping yet accessible from the sidewalk.

6.2.1 GROUND FLOOR FRONTAGES

In Mixed-Use zoning districts, ground floor use requirements are regulated by frontage type per Map 6.2-1 and Table 6.2-1. Use requirements are regulated as a percentage of the building frontage; see Figure 6.2.1.

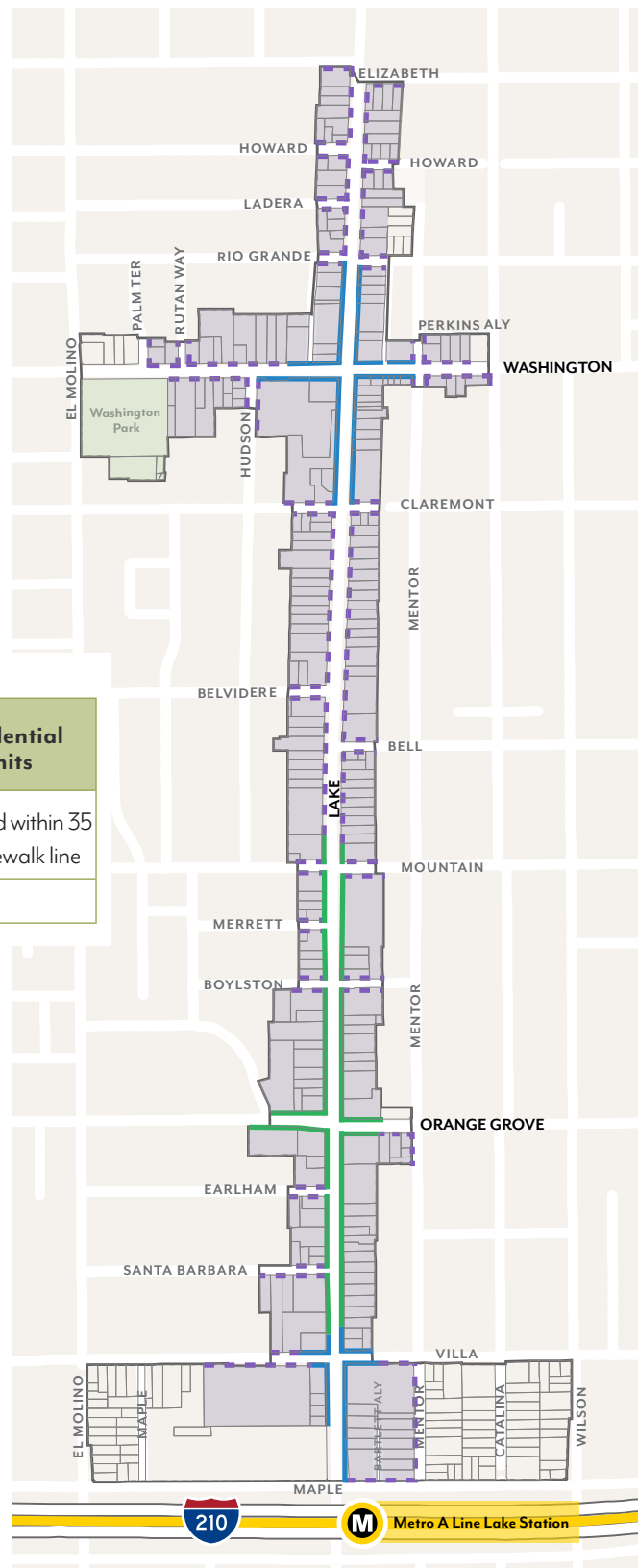
- A. **Commercial Uses.** Frontage types shall require a minimum amount of the building frontage to be comprised of, and designed for, commercial uses per Table 6.2-1 and Map 6.2-1. Permitted commercial uses by zoning district are found in Table 4.3-1.
- Entrances to non-ground floor uses, and/or entrances to uses prohibited within 35 feet of the sidewalk, shall not qualify toward the minimum commercial use percentage
 - Commercial uses shall have an average interior depth of at least 35 feet and a minimum depth of 20 feet, measured wall-to-wall.

Table 6.2-1: Allowable Ground Floor Building Frontages

Type		Commercial Uses	Residential Common	Residential Units
	1	80% min.	20% max.	Prohibited within 35 ft of sidewalk line
	2	Allowed, no percentage requirements		
	3	Allowed, no percentage requirements		

- B. **Residential Uses.** Frontage Types 1 and 2 per Table 6.2-1 and Map 6.2-1 set limitations on ground floor residential uses facing the street. Permitted residential uses by zoning district are found in Table 4.3-1.
- Types 1 and 2: Residential units on the ground floor shall be prohibited within 35 feet of the sidewalk line, inclusive of setbacks, per Table 6.2-1; see Figure 6.2-2.
 - Type 3: Residential units on the ground floor shall be permitted with direct access to the street and a minimum setback of 5 feet.
 - Residential common space on the ground floor shall be permitted per Table 6.2-1.

Map 6.2-1: Ground Floor Frontage Types



DESIGN GUIDELINES FOR VINEYARD GARDENS

One of the General Plan's Seven Guiding Principles states, "Change will be harmonized to preserve Pasadena's historic character and environment. City-wide design principles will be established so that new development blends with the old; historically and architecturally significant buildings will be preserved; new public spaces will be acquired; and we will act as stewards of our natural environment."

The North Lake Avenue corridor between Mountain and Claremont developed its historic character in the early 1900s when it was originally subdivided into approximately 45-foot-wide lots, primarily for residential use. One- and two-story single-family residential buildings were built along the street in the California Bungalow (Craftsman), Mission Revival, and Minimal Traditional styles that were popular from the 1900s through the 1940s. Although not designated as a historic district, North Lake Avenue's historic character is a valued trait of the neighborhood, and some of the general policies of the Design Guidelines for Historic Districts (2002) in Pasadena may be applied to enhance the traditional feel of neighborhood.

New development should complement and respond to the immediate area, reflecting the scale and proportion of residential buildings dating from the 1900s to 1940s in the corridor. The following guidelines are encouraged to be used in the Vineyard Gardens area.

Infill Construction:

- » The pattern in which buildings relate to the street and a sense of human scale in the neighborhood should be maintained; a front elevation should appear similar in scale to those seen traditionally in the block.
- » The height of a building should not overwhelm that of houses found traditionally on the block; a building should appear similar in mass and scale to that of single-family structures found in the neighborhood.
- » Building and roof forms should be similar to those seen traditionally. Simple rectangular building forms are preferred, and sloping roofs such as gable and hipped roofs are appropriate for primary roof forms.
- » Building materials should appear similar to those used traditionally in the neighborhood, such as wood, brick, stucco, Arroyo stone, and clay tile. Roof materials should convey a scale and texture similar to those nearby.
- » The use of architectural details that add visual interest to the street is encouraged; components should be similar in scale to those used historically. Maintain the alignment of horizontal elements along the block.
- » Windows and doors should be of a compatible size and shape, and should be placed in a similar solid-to-void relationship as historic buildings.
- » New interpretations of the three representative traditional building styles (California Bungalow, Mission Revival and Minimal Traditional) are encouraged.
- » Utilities should be placed such that their visual impacts are minimized.



Mission Revival style



Minimal Traditional style

Building Alterations:

- » Original architectural details and building materials should be preserved in place, whenever feasible.
- » Deteriorated architectural details and building materials should be repaired rather than replaced, whenever possible.
- » Original architectural details and building materials that have deteriorated beyond repair should be replaced in kind.
- » Historic windows and doors should be preserved; a new or replacement window or door should match the appearance of the original.
- » Original porches should be maintained.
- » Original forms, materials, eaves, rafter tails, gutters and other roof features should be preserved or replaced in kind.
- » An addition should have a compatible design with the original building.
- » A roof-top addition should be subordinate in scale and character.
- » Seismic retrofitting should minimize changes to original architectural features and building materials.

Streetscape/Site Development:

- » Sidewalks and driveways should be compatible with the surrounding area.
- » A fence should be in character with those used traditionally and relate to the principal structure on a lot.
- » Plant materials should be used to create continuity among properties. Use plant materials that are adapted to the Pasadena climate.
- » Maintain mature trees. Incorporate established plantings in new projects.
- » Exterior lighting should be a subordinate element.



New Craftsman-style multi-family development



Typical features of California Bungalow style

DESIGN GUIDELINES FOR VINEYARD GARDENS (CONTINUED)



Typical features of Mission Revival style



Typical features of Minimal Traditional style

6.2.2 GROUND FLOOR DESIGN

- A. **Entrances.** A minimum of one primary entrance shall be located on the primary frontage of each building and open onto a sidewalk or other public space.
1. Primary entrances shall be distinguished by architectural features or overhead projections, such as an awning or canopy.
 2. All entrances shall be recessed a minimum of 30 inches from the sidewalk line.
 3. On Washington Boulevard, entries to residential units shall be a minimum of 10 feet from the sidewalk line.
- B. **Minimum Height.** Buildings shall have a minimum ground floor height of 15 feet, measured from the sidewalk elevation closest to the primary entrance to the second story floor or roof of a one-story building; see Figure 6.2-3.
1. For non-residential and residential common space uses, the primary entrance of the first habitable floor shall be located at existing grade along the sidewalk line.
 2. For residential units, the first habitable floor shall be located between 6 feet above and 2 feet below sidewalk elevation.

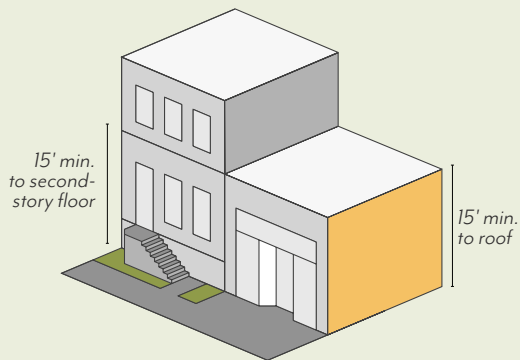


A commercial ground floor of at least 15 feet in height



A residential ground floor elevated above the sidewalk

Figure 6.2-3: Ground Floor Height



Ground floor height is measured from sidewalk elevation. Ground floor residential units may be sunken or elevated but the second story must start at least 15 feet above sidewalk elevation.

Note: Diagrams used for illustrative purposes only.

6.2.3 TRANSPARENCY

- A. **Windows & Doors.** Street-facing facades shall incorporate glass providing views into work, display, sales, lobby, or similar active areas. Minimum transparency for street-facing façades is set in Tables 6.2-1 and 6.2-2 based on use.
1. For non-residential and residential common space uses, ground floor transparency is measured as the percentage of building frontage that consists of transparent openings between a height of 2 feet and 10 feet above sidewalk elevation.
 2. All other transparency is measured as the percentage of building frontage area, viewed in elevation.
 3. Windows shall be recessed by a minimum of 2 inches from the façade; flush windows may be allowed per review authority approval.
 4. The use of color-tinted, mirrored or highly reflective glazing is prohibited.
 5. Blinds, drapes, posters, and shelving for product displays visible to the public right-of-way shall obscure a maximum of 10 percent of the transparent areas of each respective storefront or 50 percent for medical office uses.
- B. **Blank Walls.** Windowless expanses of street-facing walls shall not exceed 20 feet in length.
- C. **Security Bars.** Any exterior or interior security bars shall be designed to be fully hidden from view during business hours with devices such as concealed side pockets and ceiling cavities.



Recessed entrance with overhead awnings



Frequent entrances and windows on the ground floor

Table 6.2-2: Transparency for Non-residential and Residential Common Space Uses

	MU-C	MU-N
Ground Floor	60%	50%
Overall Façade	30%	

Table 6.2-3: Transparency for Residential Units

	All Zones
Ground Floor	15%
Overall Façade	15%

6.2.4 SHADE STRUCTURES

- A. **Shading.** Shade structures may project up to two-thirds of the sidewalk width.
1. Shade structures shall allow a minimum of 8 feet of vertical clearance from sidewalk elevation.
 2. Shade structures shall not conflict with existing trees; exceptions to the depth requirement shall be subject to review authority approval.

6.2.5 ARCADES & GALLERIES

- A. **Arcades.** Any arcades shall be located behind the minimum setback.
1. Arcades shall be a minimum of 8 feet from back of column to building façade.
 2. The distance between columns shall be equal to or greater than the arcade depth dimension, as measured from the column center.
 3. The façade within the arcade shall meet the ground floor transparency set in Section 6.2.3.
 4. Uses allowed within arcades include:
 - a. Pedestrian travel,
 - b. Seating/street furniture,
 - c. Outdoor dining,
 - d. Landscape planters, and/or
 - e. Bicycle parking.
- B. **Galleries.** Any galleries shall be located behind the minimum setback.
1. Galleries shall allow a minimum of 10 feet of vertical clearance from sidewalk elevation.

TRANSITIONAL OUTDOOR SPACES

Arcades create a shaded outdoor space between the public realm and ground floor interior. Shade structures contribute to a comfortable pedestrian experience and serve as inviting design features for ground floor uses.



Lighting and ground floor transparency all contribute to a comfortable pedestrian experience.



Awnings and arcades provide shaded space for pedestrians and outdoor dining.



Galleries cover the sidewalk, but unlike arcades, the space above is not enclosed.

6.2.6 WALLS & FENCES

- A. **Walls & Fences.** Walls and fences shall be subject to PMC 17.40.180 with the following exceptions for those located within the street setback.
1. Walls and fences shall have a maximum height of 48 inches above sidewalk elevation.
 2. Walls and fences taller than 30 inches shall be a minimum of 50 percent transparent and set back a minimum of 18 inches from the sidewalk line, separated by planted area. Walls and fences 30 inches or less in height do not have a transparency or setback requirement.
 3. Walls and fences used to enclose outdoor dining may be located at the sidewalk line and are not required to provide a planted area if the wall or fence is 36 inches or less and more than 50 percent transparent.
 4. Guardrails may exceed the maximum height to the extent required by the Building Code. The guardrail shall be a minimum of 50 percent transparent.
- B. **Stoops & Patios.** Walls along the side of a stoop, patio or entry to a residential dwelling unit greater than 30 inches in height shall be set back a minimum of 18 inches from the sidewalk line, separated by planted area.



The side of a stoop set back from the sidewalk by a planted area.

6.2.7 BALCONIES & ROOF DECKS

- A. **Balconies.** Balconies may project a maximum of 4 feet from the building façade but shall not extend beyond the sidewalk line or within 6 feet of any interior property line.
- B. **Roof Decks.** Roof decks shall be set back a minimum of 5 feet from the building edge on all sides. The sum of all roof decks on a single building shall not exceed a maximum coverage of 50 percent of the roof area.



Appropriate residential fence height and placement.



Appropriate residential fence height and placement.

6.3 Open Space

These standards are intended to:

- » Provide a variety of open space types for gathering, recreation and respite that contribute to enhanced livability within an urban setting;
- » Give residents access to natural light and fresh air in and around their living spaces;
- » Improve building design and site planning through the integration of open space throughout the development; and
- » Correlate open space requirements with number of residents and size of buildings.

TYPES OF OPEN SPACE



Private. Private Open Spaces (e.g. patios and balconies) adjoin a dwelling unit and are reserved for the exclusive use of the resident and their guests.



Common. Common Open Spaces are usable spaces shared among tenants of a building and often take the form of courtyards and pool areas. It can also include shared indoor spaces, such as lounges, community kitchens, and gyms.



Publicly Accessible. Publicly Accessible Open Spaces (e.g. plazas, pocket parks, and paseos) are privately owned but open to the public and typically include amenities such as seating, landscaping, fountains, and public art.

6.3.1 MINIMUM OPEN SPACE

- A. **Private and Common Open Space.** Projects shall provide the minimum Open Space based on use and size. Areas used regularly for parking, loading or storage shall not count towards minimum Open Space requirements.
1. **Residential.** Projects with dwelling units shall provide the minimum Open Space per Table 6.3-1 as a combination of Private and/or Common Open Space.
 2. **Non-residential.** Projects with more than 40,000 square feet of non-residential floor area shall provide a minimum of 5 percent of the gross non-residential floor area as Common Open Space.
 3. **Mixed-use.** Projects shall comply with requirements applicable to each type of use.

Table 6.3-1: Residential Open Space by Unit Type

Number of Bedrooms	0	1	2	3+
Per Unit, sq ft	125	150	200	250

- B. **Publicly Accessible Open Space (PAOS).** Projects with more than 80,000 square feet of gross floor area (GFA) shall provide 2% of GFA as PAOS.
1. PAOS shall be provided in addition to Private and Common Open Space requirements.
 2. Projects shall comply with PAOS standards per Section 6.3.4.
 - a. PAOS standards may be modified through the Design Review process, if the review authority finds that the modified design continues to be accessible to the public, functional, and includes features such as landscaping, trees, and outdoor seating.

IMPORTANCE OF OPEN SPACE

A variety of high quality, usable and accessible open space contributes to an active public realm and successful building design. A combination of **Private**, **Common**, and **Publicly Accessible Open Space** serves a range of purposes, including spaces for relaxation and community gathering for residents, employees, and visitors within an urban setting. Open spaces either on the ground floor or on upper level stories, correlated to the building use and size, can also help to break up building massing creating effective site and building design.



Private Open Space (Patio)

6.3.2 PRIVATE OPEN SPACE

- A. **Dimensions.** A minimum area of 40 square feet with a minimum dimension of 5 feet in each direction is required to qualify for Private Open Space.
- B. **Distribution.** A maximum of 40 percent of the required residential Open Space set in Table 6.3-1 shall be Private Open Space.
1. All Private Open Space shall be outdoors.
 2. Private Open Space may be located within a required setback.

6.3.3 COMMON OPEN SPACE

- A. **Dimensions.** A minimum area of 400 square feet with a minimum dimension of 15 feet in each direction is required for Common Open Space.
- B. **Distribution.** A minimum of 60 percent of the required residential Open Space set in Table 6.3-1 shall be Common Open Space shared among tenants.
 - 1. A minimum of 70 percent of Common Open Space shall be outdoors, and a minimum of 80 percent of outdoor Common Open Space shall be open to the sky.
 - 2. A maximum of 30 percent of Common Open Space may be indoors. Indoor Common Open Space shall not include spaces used primarily for circulation.
- C. **Access.** Common Open Spaces may be accessible to the public.
- D. **Hardscape.** A maximum of 25 percent of Common Open Space may be paved in standard concrete. Remaining areas shall use one of the following enhanced paving techniques: brick, natural stone, unit concrete pavers, textured and colored concrete, and/or concrete with exposed or special aggregate. Alternative paving may be allowed per review authority approval.
- E. **Landscape.** A minimum of 25 percent of Common Open Space shall be planted area with a minimum dimension of 30 inches in each direction. Landscaping shall comply with PMC 17.44.050.
- F. **Trees.** A minimum of one 24-inch box tree per project or for every 750 square feet of outdoor Common Open Space, whichever is greater, shall be planted within the Common Open Space. For projects with 2 or more trees, a minimum of 50 percent of trees planted shall be shade trees.
 - 1. Trees planted in pots on the ground floor shall not be counted towards the tree requirement.
- G. **Water Features.** A maximum of 5 percent of the required Common Open Space shall be fountains, reflecting pools, or other decorative water features. Swimming pools are not considered water features for the purposes of this standard.

CREATING COMMUNITY GATHERING SPACES

Common Open Spaces provide areas for gathering, recreation, and respite within a development.



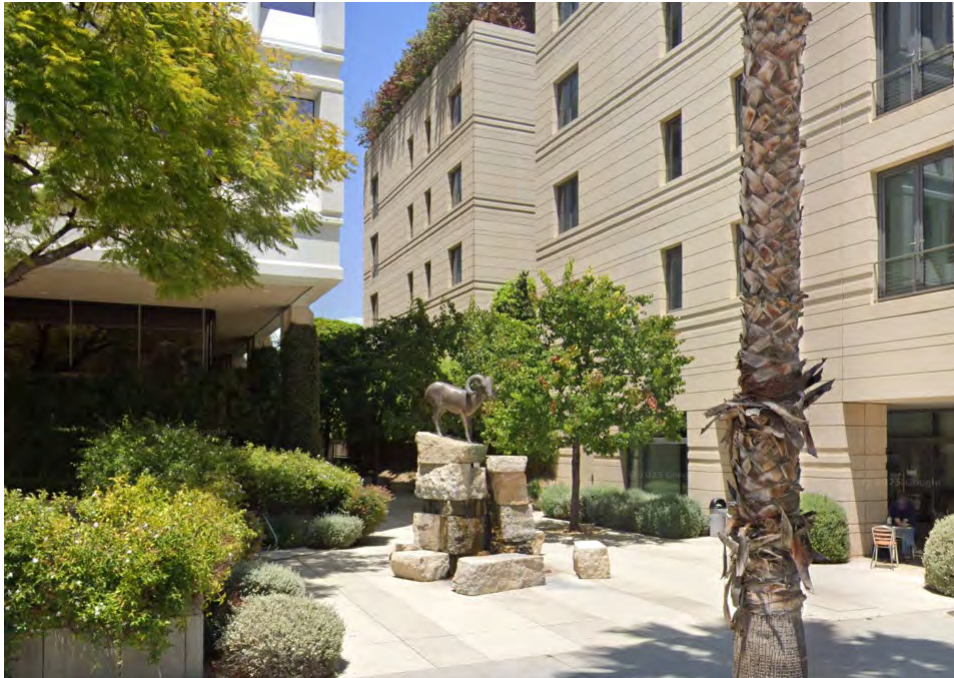
Communal picnic area with moveable seating options



Enhanced paving, seating, and landscaping

6.3.4 PUBLICLY ACCESSIBLE OPEN SPACE (PAOS)

- A. **Area.** Minimum PAOS requirements are set in Section 6.3.1.B, and may be contiguous or noncontiguous, subject to the dimension and elevation standards below.
- B. **Dimensions.** A minimum area of 400 square feet with a minimum dimension of 20 feet in each direction shall be required for PAOS.
- C. **Access.** A maximum of 20 percent of the PAOS may be used as outdoor dining for a restaurant subject to review authority approval; a minimum of 80 percent of the PAOS shall be accessible to the general public.
- D. **Signage.** PAOS shall have signage visible from the adjacent sidewalk identifying the space as a publicly-accessible amenity and listing accessible hours.
- E. **Hours.** At a minimum, PAOS shall be open to the general public from 8am to 8pm.
- F. **Elevation.** A minimum of 3,000 square feet of PAOS shall be at sidewalk elevation. If less square footage is required, then all required PAOS shall be at sidewalk elevation.
- G. **Hardscape.** A maximum of 25 percent of PAOS shall be paved in standard concrete. Remaining areas shall use one of the following enhanced paving techniques: brick, natural stone, unit concrete pavers, textured and colored concrete, concrete with exposed or special aggregate. Alternative paving may be allowed per review authority approval.
- H. **Seating.** Seating shall be provided at a minimum of 1 seat per 300 square feet of required PAOS. Fractions shall be rounded down to the nearest whole number.
 - 1. Benches shall be calculated as 1 seat per 24 linear inches.
- I. **Landscape.** A minimum of 25 percent of PAOS shall be planted area with a minimum dimension of 30 inches in length, width, and depth. Landscaping shall comply with PMC 17.44.
- J. **Trees.** A minimum of one 24-inch box tree per project or for every 750 square feet of PAOS, whichever is greater, shall be planted. For projects with 2 or more trees, a minimum 50 percent of trees planted shall be shade trees.
 - 1. Trees planted in pots, on the ground floor, shall not be counted towards the tree requirement.
- K. **Common Open Space Credit.** PAOS in excess of the minimum may count towards a maximum of 30 percent of the Common Open Space requirement at a 1:1 ratio.



Pedestrian paseos should have doors and windows facing them to improve accessibility and safety.

GUIDELINES FOR PUBLIC OPEN SPACES

The following guidelines are intended for any publicly-accessible open spaces, including plazas, paseos, and pocket parks, required or otherwise.

Framing & Dimensions

- » Walls facing the open space should adhere to façade modulation standards defined in section 6.1.7.
- » In addition to meeting the parking standards defined in section 6.4, parking lots or structures facing the open space should be screened with landscaping or creative, pedestrian-friendly architecture.
- » Storefronts (commercial), and unit entries or stoops (residential) should front onto the open space, where possible.
- » Design open spaces to maintain direct sight lines between opposite entrances, where possible. If paseos are required to jog due to project constraints, maintain angled views to indicate it is not a dead end, or manage jogs through wayfinding and lighting to increase safety.
- » A mix of direct sunlight and shade should be provided through shade structures, landscaping, and building massing.
- » The design of connector paseos should consider the width of the paseo to height of the building to manage pedestrian scale and a sense of enclosure. Recommended proportion is 1:2.5 (width: adjacent building height), where possible.



Public paseos provide walkable connections, and can offer opportunities for outdoor dining. Landscaping and trees provide shade and improve aesthetics.



Public open space with retail storefronts and pedestrian-scale lighting.

Accessibility & Safety

- » Open spaces should include a sufficient amount of lighting for safe and comfortable night-time use.
 - Lighting should be an integral component of the overall open space design and is encouraged to be incorporated in public art.
 - Lighting should be pedestrian scaled, including both low-level pathway lighting and overhead wall mounted fixtures.
- » Storefronts, stoops, and other entries facing the open space should be designed and lighted to prevent hiding spaces.

Amenities & Programming

- » Open space should include at least one special feature such as a public art piece or water feature.
- » Bike racks and scooter parking areas should be provided near entrances, without obstructing walkways.
- » Non-transactional programming should be encouraged to activate the open space without financial barriers to entry.

6.4 Parking

These standards are intended to:

- » Reduce the visual impacts of parking;
- » Regulate appropriate parking supply and location in a manner that prioritizes pedestrian access and multi-modal activity;
- » Encourage change of use and adaptive reuse of existing buildings through parking reductions and exemptions;
- » Promote a more efficient use of space through shared parking among multiple uses within a project; and
- » Increase design standards for parking structures by ensuring habitable floor area and screening between parking and street frontage.

IMPORTANCE OF PARKING

Vehicle parking access, location and supply influences the street environment, multi-modal travel and overall development. Managing the location of vehicular access, such as entries and driveways, can help to promote continuous sidewalk activity and safer travel across modes. Similarly, minimizing surface parking lot size and locations (such as placing lots behind buildings or a landscaped open space), supports the success of street-fronting activity, such as pedestrian travel and commercial frontages. The number of required parking spaces is another defining factor that shapes urban travel and development. By establishing an appropriate number of parking spots by land use and size of development, residential and commercial activity can be supported while also attracting a variety of new development. Through tailored standards as well as parking reductions and exemptions for certain uses and conditions, space efficiency and cost savings are promoted.

6.4.1 MINIMUM PARKING

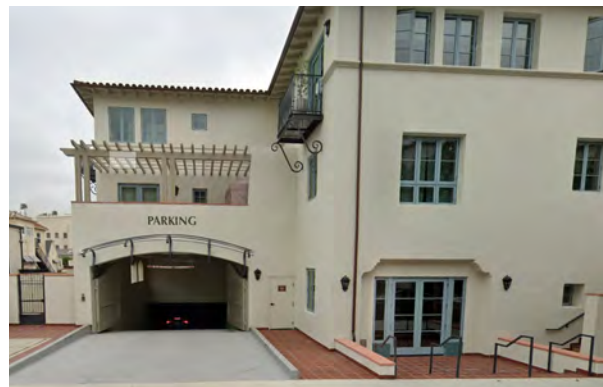
- A. **Number of Spaces.** Projects shall provide off-street automobile parking spaces per Table 6.4-1 based on general use classifications.
 1. For projects within one-half mile of a Metro station, reductions in parking and a maximum number of parking spaces shall apply per PMC 17.50.340.
 2. For projects within 500 feet of a Metro station, projects may be permitted up to a 30% reduction in the number of parking spaces.
 3. Bicycle parking shall be required per PMC 17.46.320.
- B. **Shared Parking.** Parking may be shared among multiple uses per PMC 17.46.050.
- C. **Unbundled Parking.** For any building with new residential units, off-street automobile parking spaces shall be leased or sold separately from the unit rental or purchase fees, such that renters or buyers have the option of renting or buying the residential unit at a lower price than if the parking was included.
 1. For deed-restricted affordable units, one parking space shall be included in the base rent of each unit. The tenant may choose to receive the parking space or receive a rent discount equivalent to half the amount charged for monthly lease of a parking space. Tenants of affordable units shall not sublease their parking spaces.
 2. Renters or buyers have the right of first refusal to parking built for their unit. Any remaining spaces may be leased to other users on a month-to-month basis. New occupants shall have the opportunity to lease or purchase parking built for their unit.
 3. Unbundled parking is not required for a residential property or unit with an individual garage that is functionally a part of the property or unit (e.g., certain townhome and row houses).

Table 6.4-1: Minimum Parking by Land Use

Use Classification ¹	Number of Spaces	Exceptions
Residential	≤1-bed: 1 per unit ≥2-bed: 1.5 per unit Guest: 1 per 10 units	Guest parking may be shared with commercial parking in mixed-use projects ²
Live/Work Units	1.5 per unit	
Recreation, Education & Public Assembly	PMC 17.46.040	
Office, Professional & Business Support	2 per 1,000 sf	No parking required for: • First 5,000 sf of project; and • First 500 sf of outdoor dining per tenant
Retail Sales (including Restaurants)		
Services (excluding Lodging)		
Lodging	0.5 per room	No parking required for first 15,000 sf of banquet space
Industry, Manufacturing & Processing	2 per 1,000 sf	Recycling Centers: plus 1 space per bin
Transportation, Communications & Utility	PMC 17.46.040	
Other Exceptions		
No new parking required for: • Projects within designated historic resources (excluding additions) • Changes of use in structures built prior to 1970		
¹ Use classifications correspond to general use categories in Table 4.3-1. The number of spaces listed above shall apply to all uses listed under these general categories, with the exception of specific uses where the parking requirement is lower per PMC 17.46.040.		
² No shared parking agreement is required; each guest space shall count as 1 commercial space.		

6.4.2 VEHICLE ACCESS

- A. **Driveways.** For Projects with less than 200 feet of primary street frontage, a maximum of 1 two-lane driveway shall be permitted. For sites with more than 200 feet of primary street frontage, a maximum of 2 two-lane driveways shall be permitted.
1. Driveways shall be prohibited on primary frontages of 200 feet or less where there is access from a secondary street or alley.
 2. The Zoning Administrator shall determine the primary frontage.
- B. **Gates.** Controlled entrances to parking shall be located a minimum of 20 feet from the property line to allow for a queuing vehicle without blocking the public right-of-way.
1. Gates at parking entrances shall be designed to conceal associated mechanical equipment from the public right-of-way in compliance with PMC 17.40.150.



Entrances to structured parking should be integrated into façade design and screen upper portions of the entrance not required for vehicle clearance.

6.4.3 LAYOUT & DESIGN

- A. **Surface Parking.** Parking lots shall comply with PMC 17.46.230 with the following exceptions:
1. Parking shall be set back a minimum of 30 feet from the primary frontage, a minimum of 10 feet from any secondary frontage, and a minimum of 5 feet from RM/RS zoning.
 2. Parking shall be buffered by permitted non-parking uses or a landscaped setback adjacent to the sidewalk line, except for driveways or pedestrian access to the parking area.
 - a. Landscaped setbacks shall include a row of hedges or shrubs with a minimum height of 3 feet at the time of planting that form a continuous visual screen to block vehicle headlights.
- B. **Entrances to Structured Parking.** For structured and subterranean parking, vehicular entrances shall employ the same materials and architectural style as the primary building.
1. Exterior building materials shall wrap into parking entrances/exits for a minimum of 20 feet from the building façade, except areas not visible from public streets.
 2. Entrances shall not be larger than the necessary clearance area.
- C. **Structured Parking.** Along primary frontages, all floors of parking structures shall be lined with non-parking uses (e.g. commercial or residential) for a minimum of 35 feet in depth. Pedestrian access and driveways in compliance with Section 6.4.2 are excluded from this requirement.
1. Parking structure façades visible from public streets, excluding alleys, shall employ the same materials and architectural style as the primary building.
 - a. Open areas on the façade shall be designed as windows or screened using heavy-gauge metal, precast concrete panels, laminated glass, green walls, photovoltaic panels or other material per review authority.
 2. Vehicular ramps shall not be located within 10 feet of the sidewalk line, except at parking entrances/exits.
 3. A parking structure shall not exceed the height of the tallest building it serves.
- D. **Underground Parking.** Subterranean parking shall be set back a minimum of 5 feet from RM/RS zoning. Otherwise, it may extend up to the property line.



Materials do not change at this parking entrance; the same architectural style should be employed throughout.

Ch. 7

Implementation & Administration

Implementation & Administration	119
7.1 Implementation Actions	120
7.2 Citywide Implementation Overview	127
7.3 Funding	136
7.4 Infrastructure	141
7.5 Administration	144





Implementation & Administration

CHAPTER OVERVIEW

The NLSP will be primarily implemented through the adoption of the plan's new Land Use, Public Realm, and Development regulations, which align private sector investment with the plan's vision, goals, and policies. In addition, public sector improvements and programs funded through the City and outside sources, can further implement the plan, particularly where redevelopment is less likely to occur.

This chapter includes targeted implementation actions intended to help guide and prioritize the implementation of the plan. It also provides an overview of existing services, plans, and programs, all of which can be leveraged to help implement the plan. A summary of funding sources is also included to guide the City in understanding and selecting available funding sources to implement the improvements and programs identified in the NLSP. Infrastructure facilities for transportation and traffic, wastewater, water supply,

solid waste, stormwater, and electricity are also identified in this chapter for the purposes of meeting the anticipated growth. This chapter concludes with plan administration. This chapter is organized into the following sections:

- » **7.1. Implementation Actions**
- » **7.2. Citywide Implementation Overview**
- » **7.3. Funding**
- » **7.4. Infrastructure**
- » **7.5 Administration**



7.1 Implementation Actions

Specific plans are used by various City departments to review projects, seek funding, and to understand the vision, goals, and policies of specified geographic areas to guide improvements and programming. Implementing specific plans requires collaboration across City departments and coordination with existing citywide implementation programs, plans, and efforts. See Section 7.2 for an overview of Citywide programs that may intersect with implementation of this Specific Plan.

The following implementation actions are intended to guide the City in implementing the NLSP over time with generalized timeframes as follows:

- » **Ongoing:** Periodic or on a continuing basis
- » **Immediate:** Upon adoption of the Specific Plan
- » **Near-Term:** 0-5 years
- » **Medium-Term:** 5-10 years

As changes in City priorities, economic conditions, and market trends occur over time, the City may need to revisit and reprioritize the implementation actions. Table 7.1-1 and the following sections outline implementation actions for the NLSP, including description, timeframe, and responsible parties for each action. Information included for each action is intended to help guide the City in taking next steps, which will include additional planning, coordination, community input, and public processes.

RESPONSIBLE DEPARTMENTS:

- » **P&CDD:** Planning & Community Development Department
- » **DOT:** Department of Transportation
- » **PWD:** Public Works Department
- » **A&CAD:** Arts & Cultural Affairs Division
- » **EDD:** Economic Development Division
- » **PR&CS:** Parks, Recreation and Community Services
- » **PWP:** Pasadena Water and Power

Table 7.1-1: North Lake Specific Plan Implementation Actions

Action	Description	Timeframe	Departments
Amendments (A)			
A-1: General Plan Map and Text Amendment	Amend General Plan Land Use Diagram to adjust NLSP boundary and update land use designations per Chapter 4 (Land Use).	Immediate	P&CDD
A-2: Zoning Code Map and Text Amendment	Amend the Zoning Code to replace existing development standards with the development standards provided in Chapters 4-6 of this Specific Plan. Amend Zoning Map to replace zoning district designations indicated on the Zoning Map with the new NLSP zoning districts.	Immediate	P&CDD
A-3: Specific Plan Amendment	Amend the NLSP including new goals and policies, as well as land use and development standards.	Immediate	P&CDD
A-4: Annual Report	The Planning Department will present an annual report to the Planning Commission on the implementation actions in Table 7.1-1 to inform the Capital Improvement Program and the overall progress of the plan.	Ongoing	P&CDD DOT PWD

Action	Description	Timeframe	Potential Funding Sources/ Relevant CIP Programs	Depts.
Infrastructure, Mobility, and Sustainability (IMS)				
IMS-1: Master Street Tree Plan	Review street tree palette from the Master Street Tree Plan to consider tree designations that better address Specific Plan and Climate Action Plan objectives for climate resilience, shading, urban cooling, and carbon sequestration and which are complementary to adjacent development and uses. [Refer to A.2.]	Near-Term	General Fund	PWD
			Street Tree Replacement Deposit	
			CIP: • Tree Replacement/Planting Citywide FY 2025-2029 (78959) ¹ • Street Tree Master Plan ²	
IMS-2: North Lake Traffic & Pedestrian Safety Enhancement Project	Advance the North Lake Traffic & Pedestrian Safety Enhancement Project to make walking along the corridor easier, safer, and more interesting. Implement pedestrian-oriented capital improvements such as continental crosswalks, curb extensions, pedestrian level lighting, leading pedestrian intervals, and strategies such as water-conserving low-impact development (LID) landscaping and smart irrigation techniques.	Near-Term	Gas Tax	P&CDD DOT PWD PWP
			Traffic Reduction Fee	
			CIP: • Citywide Complete Streets Program FY 2025-2029 ³ • ADA Improvements Programs (73944, 73937, 73913, 73938) ⁴ • Installation of Leading Pedestrian Interval and Audible Signal and Signalized Intersections (75125) ⁵ • Citywide Continental Crosswalk Implementation (75917) ⁶ • Lake Avenue Traffic and Pedestrian Safety Enhancements from Mountain St. to Maple St. (75105) ⁷	
IMS-3: Complete Streets Program Improvements	Identify opportunities for safety and mobility improvements consistent with DOT's Complete Streets Program, such as signalized pedestrian crosswalks and bulb-outs with sustainable elements like bioswales. Projects should incorporate cooling strategies, such as green infrastructure, cool pavements, shade trees, and cooling amenities aligned with the City's Cooling Pasadena Program objectives.	Near-Term	Gas Tax	DOT PWD P&CDD
			Traffic Reduction Fee	
			CIP: • Citywide Complete Streets Program FY 2025-2029 ³ • ADA Improvements Programs (73944, 73937, 73913, 73938) ⁴ • Citywide Leading Pedestrian Interval/ Accessible Pedestrian Signals Implementation Program FY 2025-2029 ⁵ • Citywide Continental Crosswalk Implementation (75917) ⁶	

Action	Description	Timeframe	Potential Funding Sources/ Relevant CIP Programs	Depts.
IMS-4: Pedestrian Transportation Action Plan (PTAP)	Utilize the PTAP to advance pedestrian-oriented public realm enhancements such as pedestrian refuges, High-Intensity Activated Crosswalks (HAWKs), or pedestrian traffic signals (where appropriate) along North Lake Avenue. Develop a streetscape plan, with community input, to establish a unique sense of place, provide pedestrian amenities, and support wellness through walking and biking.	Near-Term	CIP: • Citywide Complete Streets Program FY 2025-2029 ³ • ADA Improvements Programs (FY 2025, FY 2026-2030, 73944, 73937) ⁴ • Citywide Leading Pedestrian Interval/Accessible Pedestrian Signals Implementation Program FY 2025-2029 ⁵ • Pasadena Transportation Action Plan - Outreach and Conceptual Design (75511) ⁸ • Citywide Continental Crosswalk Implementation (75917) ⁶ • Installation of Pedestrian Hybrid Beacons (HAWKs) at Various Locations ⁹	PWD DOT
IMS-5: Mobility Element Update	As a part of the next Mobility Element update, reconsider the Street Type designation for North Lake Avenue to evaluate its function as a Crosstown Connector and impact on surrounding neighborhoods and streets.	Medium-Term	General Fund	P&CDD DOT
IMS-6: Streetscape Program for North Lake Avenue	Develop a streetscape plan to moderate vehicle speeds and make North Lake Avenue and other streets in the NLSP area safer and more pedestrian-friendly to accommodate activities such as people-watching, street vending, street or farmers markets, street furniture, public art, murals pedestrian-scaled lighting, and bicycle facilities, and other amenities. As a part of the streetscape plan, study the feasibility of eliminating on-street parking in front of drive-through businesses on North Lake Avenue from Orange Grove Boulevard to Mountain Street; the elimination of left-turn bays; and consider right-of-way reallocation and widening the central median for community use through traffic analysis and community input.	Near-Term	Commercial Development Fee	DOT PWD P&CDD
			Community Development Block Grant	
			CIP: • ADA Improvements Programs (FY 2025, FY 2026-2030, 73944, 73937) ⁴ • Pasadena Transportation Action Plan - Outreach and Conceptual Design (75511) ⁸	

Action	Description	Timeframe	Potential Funding Sources/ Relevant CIP Programs	Depts.
IMS-7: Citywide Active Transportation Plan (ATP)	Analyze bike facilities that are balanced with the street function of North Lake Avenue. Identify, evaluate, and prioritize walking and biking routes and improvements to major transit stops including the Metro A Line Lake Avenue Station. Key bicycle lane transit connections to be considered for enhancements are the existing standard bicycle lanes on Maple Street and Corson Street which will be evaluated for the appropriateness of upgrading the bike lanes to either Class II bicycle facilities with a buffer or Class IV protected bicycle facilities.	Medium-Term	ATP	DOT
IMS-8: Bicycle Transportation Action Plan (BTAP)	Support DOT implementation of bicycle infrastructure in alignment with the BTAP, including a Buffered Bike Lane along Orange Grove Boulevard and Greenways along Villa Street, El Molino Avenue, and Wilson Avenue. Features for the Greenways to improve access and safety for walking and biking include traffic calming treatments such as speed tables, curb extensions, and traffic circles. The community will select the specific type of traffic calming features that would be the most appropriate for the neighborhood context. Also, explore future facilities to provide connections to existing bicycle facilities. When funding becomes available, use concept corridor plans to develop design packages for construction bidding to implement the improvements.	Medium-Term	ARPA	DOT
			ATP	
			Gas Tax	
			Measure R	
			Transportation Fund	
			MTA Grant	
			Private Capital	
			Sewer Fund	
			Surface Transportation Program	
			TDA Article 3	
			Traffic Reduction Fee	
			CIP: • Citywide Complete Streets Program FY 2025-2029 ³ • Greenway (Bike Boulevard) Implementation (75126) ¹⁰ • Pasadena Bicycle Program FY 2021-2025 (75111) ¹¹	
IMS-9: Short Range Transit Plan and Bus Stop Improvement Program	Identify bus stop enhancements and amenities such as shelters, lighting, seating, and real-time transit information along North Lake Avenue.	Medium-Term	CIP: • Bus Stop Improvement Program (75900) ¹²	DOT P&CDD

Action	Description	Timeframe	Potential Funding Sources/ Relevant CIP Programs	Depts.
IMS-10: Neighborhood Traffic Management Program	Initiate a Neighborhood Traffic Management Program to involve residents and community members in closing Boylston Street between North Lake Avenue and Mentor Avenue.	Near-Term	CIP: • Citywide Neighborhood Traffic Management Program FY 2021-2025 (75114) ¹³	DOT
IMS-11: Recreation and Parks Facilities	Consider identifying locations for future recreational and park facilities in and around the NLSP area as part of an updated citywide recreation and park facilities needs assessment to align facilities with future population.	Medium-Term	CIP: • Citywide Parks Maintenance Program FY 2025 - 2029 ¹⁴	PR&CS
Community Identity, Programming and Public Art (PA)				
PA-1: Placemaking Program	Establish a placemaking program to work with residents, businesses, and private investors, and other City departments to support the development and installation of capital improvements and public realm improvements and their viability over time. Work with Housing and Economic Development to support housing opportunities and community-serving retail to support residents.	Medium-Term	General Fund	TBD
PA-2: Citywide Rotating Public Art Program	Consider placing temporary public art within the plan area as part of the Citywide Rotating Public Art Program.	Ongoing – Medium-Term	Rotating Public Art Exhibition Program Cultural Affairs Annual Grant Program	A&CAD
PA-3: Pedestrian- Oriented Art in Public Realm	Explore opportunities for pedestrian-oriented art on commercial and mixed-use portions of streets in the plan area through artist-designed crosswalks, utility boxes, and murals, as well as enhancements to blank facades, light poles, medians, and parking strips.	Near-Term	Public Art Program	A&CAD
PA-4: Temporary Art Installations in Empty Storefronts	Connect building owners with arts organizations to develop new temporary art installations in empty storefronts along streets in the plan area.	Near-Term	BID Membership Fees Cultural Affairs Annual Grant Program	A&CAD EDD

Action	Description	Timeframe	Potential Funding Sources/ Relevant CIP Programs	Depts.
PA-5: Business Association Formation	Support the formation of a business and/or property-owner association such as a Business Improvement District (BID) to strengthen opportunities for placemaking, branding and marketing, district signage, public art, parking management, and façade improvement programs along North Lake Avenue. Consider the preparation of an economic analysis of the North Lake Avenue corridor to assist in guiding City and BID investment.	Near-Term	BID Membership Fees	EDD
PA-6: Parking Program	Explore establishing a Parking District, use of parking meters, or other financing mechanisms and sources to plant and maintain street trees.	Medium-Term	BID Membership Fees	EDD
PA-7: Gateway to the NLSP Area	Explore opportunities for creating signage, such as a monument, that would act as a gateway to the NLSP area.	Medium-Term	General Fund	DOT, P&CDD, PWD, PR&CS

GLOSSARY:

- » **AHSC:** Affordable Housing and Sustainable Communities Program (CA State Funding)
- » **ARPA:** American Rescue Plan Act of 2021 (Federal Funding)
- » **ATP:** Active Transportation Program (CA State Funding)
- » **BID:** Business Improvement District
- » **CIP:** Pasadena Capital Improvement Program (adopted 2025-2030 and recommended 2026-2030 programs)
- » **MTA:** Los Angeles County Metropolitan Transit Authority
- » **TDA:** Transportation Development Act (CA State Funding)

¹ This project will provide for the replacement, planting, and watering of public trees citywide. It will allow City staff to apply for grant funding to help finance the planting and watering of trees that need to be replaced.

² This project will provide for updating the City's Street Tree Master Plan and the street tree inventory.

³ The Citywide Complete Streets Program is focused on developing projects that enhance major corridors and collector streets to increase multimodal options and reduce reliance on passenger vehicles. Enhancements are intended to increase safety for persons walking, rolling (bicycles and scooters), and transit riders.

⁴ These projects include the Annual Citywide Street Resurfacing and ADA Improvement Program FY 2025 (73944), the Annual Citywide Street Resurfacing and ADA Improvement Program FY 2023/2024 (73938), the Curb Ramp ADA Improvements Program FY 2022 - 2026 (73937), and the Annual ADA Citywide Sidewalk Improvement Program (73913). The ADA Improvement Program provides for the systematic annual rehabilitation of streets throughout the City including reconstruction, resurfacing and/or slurry of asphalt pavement; improvements to concrete curbs and gutters; improvements to sidewalks; and, installation of ADA-compliant ramps along renovated streets. The Curb Ramp ADA Improvements Program provides for the installation of new curb ramps along arterial, residential, and collector streets that currently lack curb ramps in order to eliminate the City's backlog of over 600 high priority locations. The Annual ADA City Sidewalk Improvement Program provides for annual concrete sidewalk improvements citywide, prioritized by the City's Accessibility and Disability Commission's criteria.

⁵ This project provides for the installation of leading pedestrian interval and audible pedestrian signals at approximately 70 signalized intersections, including along the Lake Avenue corridor.

⁶ This project provides for the systemic replacement of existing marked crosswalk striping with continental style crosswalks at approximately 340 signalized intersections and 70 marked uncontrolled crosswalks citywide.

⁷ This project provides for the installation of pedestrian and safety enhancements for the North Lake Avenue corridor from just north of Maple Street to Mountain Street. These enhancements include: shortening pedestrian crossing distances with bulb-outs at key intersections; widening sidewalks where possible; installation of improved or new pedestrian lighting; installation of leading pedestrian signals; and high-visibility crosswalks and other upgrades.

⁸ This project provides for public outreach and conceptual design for corridors identified in the Pedestrian Transportation Action Plan (PTAP).

⁹ This project provides for the installation of High-Intensity Activated Crosswalks (HAWKS) at various locations throughout the City, including at the intersections of Lake Avenue and Elizabeth Street and Lake Avenue and Washington Boulevard.

¹⁰ This project provides for the design of enhancements to the accessibility and safety of the bicycle network along four north-south corridors in Pasadena, including segments of El Molino and Wilson Avenues.

¹¹ This project provides for the implementation of the Bicycle Transportation Action Plan, additional on-street bike parking, the retrofit of existing bikeway facilities to meet current Caltrans standards, and the implementation of a bicycle safety and outreach program. The project will also look at closing gaps in the bike lane network and improving existing facilities.

¹² This project provides for the installation of new bus benches, bus stop amenities, and concrete paving at various bus stops throughout the City.

¹³ This program is a comprehensive process for managing traffic volumes, travel speeds, and traffic-related noise in the City's residential neighborhoods. Ongoing work, including speed hump investigations, the preferential parking program, and the Neighborhood Traffic Management Program (NTMP) will continue in various neighborhoods in FY 2025. A specific NTMP for the neighborhood of Mountain Street/El Molino Avenue (between Los Robles Avenue and Lake Avenue) is expected to begin in FY 2026.

¹⁴ This project provides for a program to address and ensure maintenance and repairs are regularly scheduled and performed on various park amenities.

7.2 Citywide Implementation Overview

The City of Pasadena currently provides a wide variety of services and programs either directly or through partnerships with local non-profits, many of which can support the implementation of Specific Plans. Services and programs relevant to the NLSP that are implemented through citywide methods are listed in the following section. Learn more about the organization of the City of Pasadena, including how various City departments are structured and connected in Figure 7.2-1.

EQUITABLE IMPLEMENTATION

Today's cities have a responsibility to acknowledge the harm of redlining and other discriminatory policies, and plan for future development with consideration to the persisting impacts of historic disinvestment. Such implications may include displacement through prohibitive increases in housing costs or commercial rents, or discriminatory leasing practices in response to new interest and investment from higher-income and non-minority populations.

While the Specific Plan cannot directly implement affordability requirements or tenant protection policies, the land use and design standards in this document intend to benefit all community members by allowing a variety of housing types, restricting inappropriate uses, providing more parking flexibility to support small business, and requiring developers to implement more public realm improvements. The Specific Plan will supplement other City policies and initiatives to help maintain affordability and strengthen existing community resources.



1. General Fund

WHO: Mayor & City Council

WHAT: Primary fund of the City that is used to account for all general revenues of the City not specifically levied or collected for other City funds and for expenditures related to the rendering of general services by the City. Operating and capital budgets are created using guiding principles to determine budget priorities.

WHEN: Every year the City Council adopts an Operating Budget allocating resources to fund vital public services and programs for everyone who lives, works and plays in the City of Pasadena.

LEARN MORE HERE:

<https://www.cityofpasadena.net/finance/general-fund/>

2. Capital Improvement Program

WHO: Department of Public Works

WHAT: The City appropriates annual capital funds by department and project category through the Capital Improvement Program (CIP). The CIP budget consists of projects aimed at improving the city's public infrastructure such as streets, transportation issues, street lights, traffic signals, parks, public buildings, sewer and storm drains, the Rose Bowl, the Pasadena Convention Center, technology, and water and power projects. Projects can be short, medium, or long-term.

WHEN: The CIP Budget is submitted annually to the City Council as a separate budget document in order to provide more detailed descriptions of City Capital Improvement Projects scheduled to take place over the course of the 5-year lifetime of the document.

LEARN MORE HERE:

<https://www.cityofpasadena.net/public-works/engineering-and-construction/capital-improvement-program/>

3. Master Street Tree Plan

WHO: Urban Forestry Program, Department of Public Works

WHAT: Serves as the guiding document that designates the official tree species to be planted on a block-by-block basis throughout the City. The goal of the Master Street Tree Plan (MSTP) is to promote a uniform urban design on a neighborhood scale, while also promoting species diversity city-wide. With the development and expansion of the City, and with changes in arboricultural practices, the MSTP has been revised and amended accordingly.

WHEN: Periodically



LEARN MORE HERE:

<https://www.cityofpasadena.net/public-works/urban-forestry/#master-street-tree-plan>

4. Tree Protection Ordinance

WHO: Urban Forestry Program, Department of Public Works

WHAT: The City Trees and Tree Protection Ordinance was adopted as Chapter 8.52 of the City's Municipal Code in 2002. The Tree Protection Ordinance includes measures to protect four categories of trees including (1) public trees, (2) landmark trees, (3) native trees, and (4) specimen trees in certain areas of the City. The process for designating landmark trees is included in the ordinance, in addition to requirements for removal and pruning of protected trees. The ordinance also includes Tree Protection Guidelines that seek to avoid negative impacts to protected trees that may occur during construction. If provisions are violated, the ordinance outlines penalties and administrative proceedings.

WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/public-works/urban-forestry/#pasadena-tree-ordinance>

5. Complete Streets Program

WHO: Department of Transportation

WHAT: Implements Assembly Bill 1358, known as the Complete Streets Act, enacted in 2008, to reduce greenhouse gas emissions, make the most efficient use of urban land and transportation infrastructure, and improve public health through shifting short trips from automobiles to biking, walking and use of public transit. The Mobility Element (2015) of the City's General Plan guides the Department of Transportation (DOT) through goals and objectives that address complete streets. DOT implements Complete Streets through the Pasadena Street Design Guide (2017), and the Bicycle Transportation Action Plan (2015). DOT is working on several transportation and safety projects to address complete streets and eliminate fatalities and serious injuries on streets.



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/transportation/>

6. Water Conservation, Recycling, Stormwater Management

WHO: Department of Water and Power

WHAT: Pasadena Water and Power (PWP) is a community enterprise that provides electricity and water to the Pasadena community. The PWP General Manager reports to the City Manager and is governed by the City Council. The Urban Water Management Plan (2021) provides an analysis of long-term water supply and demand planning for PWP, including system analysis, reliability assessment, water-use targets, water shortage contingency planning, demand management and climate change impact.

WHEN: Prepared every five years in compliance with the Urban Water Management Planning Act (California Water Code Sections 10610 through 10656).

LEARN MORE HERE:

<https://pwp.cityofpasadena.net/uwmp/>

7. Energy and Energy Efficiency

WHO: Department of Water and Power

WHAT: Through the Power Integrated Resource Plan (IRP), Pasadena Water and Power (PWP) sets steps for upholding local, state and federal mandates and internal power supply goals, including having a balanced and sustainable mix of sources towards a green portfolio in the future. Renewable energy sources include solar, wind, geothermal, landfill gas, and hydropower. As part of energy efficiency and sustainability, PWP also has several programs and initiatives including electric vehicles, solar, green power, greywater, drought-tolerant landscaping, and enhancing Pasadena's watershed.

WHEN: Ongoing

LEARN MORE HERE:

<https://ww5.cityofpasadena.net/water-and-power/power>

pwp.cityofpasadena.net/sustainability/

8. Climate Action Plan

WHO: Planning & Community Development Department, Department of Public Works, Department of Transportation and Department of Water and Power

WHAT: Provides a strategic framework for measuring, planning, and reducing the City's share of greenhouse gas (GHG) emissions with a goal of reducing emissions by more than half by the year 2035. The City is working on a variety of programs and projects to address climate change and reduce GHG emissions to implement the CAP, including the Cooling Pasadena Program, which is currently under development to prepare a toolkit and to identify strategies to cool Pasadena's streets, the Complete Streets Program, and the Save Water Program.



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/planning/planning-division/community-planning/climate-action-plan/>

9. Development Impact Fees

WHO: Planning & Community Development Department

WHAT: The City charges development impact fees on new development to offset the cost of public facilities related to the development, in turn helping to fund implementation actions such as improvement projects. While impact fees associated with new development are updated periodically, current fees include Public Works' Residential Impact Fee to fund affordable housing, Department of Transportation's Traffic Reduction & Transportation Improvement fee, Public Works' Sewer Facility Charge, and Arts and Cultural Affairs Division's fees to fund public

art. Pasadena's development impact fees are calculated based on the number of bedrooms or gross built area (for the residential and transportation fees) or estimated project value (for public art fees). Impact fees are directed to the General Fund, which funds initiatives in the associated fee categories.

WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/planning/permit-center/fee-schedules/>

10. Parks, Recreation and Community Services

WHO: Parks, Recreation and Community Services

WHAT: Provides the City with recreational and human service programs focused on preserving and improving the physical, social, and economic health of Pasadena neighborhoods. The parks and recreation portion of the Department is guided by the City's General Plan Green Space, Parks and Recreation Element and Master Plan (2007), which work together to assess existing facilities and programs, identify additional needed parking facilities or recreation programs, and recommend best methods to meet needs. New park projects, including planning and design studies for new parks and the construction of green spaces, facilities, and community centers are funded in part through the CIP, in addition to other funding sources.



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/parks-and-rec/>

11. Public Art Program

WHO: Arts and Cultural Affairs Division

WHAT: The Public Art Program focuses on building a publicly available collection of contemporary art. The Cultural Nexus Plan (2004) and the Public Art Master Plan (2014) guide the Public Art Program through established cultural policies and a vision for new public art development in Pasadena, supported by goals and objectives with strategies for implementation. The City's Public Art Program includes Public Art Requirements that focus on two areas: new private development and City construction (CIP) projects. The requirements may be satisfied by the creation of a site-specific public art or by payment in-lieu of artwork. In addition, the Public Art Program includes a Rotating Public Art Exhibition Program that complements the permanent artworks commissioned by the City's Public Art Requirements by temporarily installing contemporary sculptures in each of Pasadena's seven Council Districts.



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/planning/arts-and-cultural-affairs/>

12. Economic Development Division

WHO: Economic Development Division

WHAT: The Economic Development Division spurs and facilitates citywide economic opportunities and strives to secure ongoing local investment that sustains a healthy job market, provides high quality commercial districts, and creates stable tax revenues. The Division's strategic plan, completed in the summer of 2024, focuses on six guiding principles which will guide the division's priorities, programs, and services for the next five years.

1. Build Systems And Partnerships For Success
2. Champion Life Sciences and Cultivate a Deep Tech Ecosystem

3. Invest in Neighborhoods and Small Businesses
4. Reinforce Pasadena as a Destination for Culture and Entertainment
5. Expand and Streamline Workforce Opportunities
6. Prioritize Place and Accessibility Investments

WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/economicdevelopment/>

13. Master Plans

WHO: Planning & Community Development Department

WHAT: Some areas of the City are governed by existing Master Development Plans. These plans set forth the rules for development on property owned by major public institutions in Pasadena and are the implementation tools of the General Plan in these areas. The Master Plans set forth the maximum amount, type, and location of future development which will occur for the institution during the lifespan of the Master Plan. In the NLSP area the existing Master Plan is the Lake Avenue Congregational Church (approved December 1985). As areas regulated through Master Plan documents, the Lake Avenue Congregational Church is separate from the NLSP, however, this institution is an important community gathering place in the NLSP area.

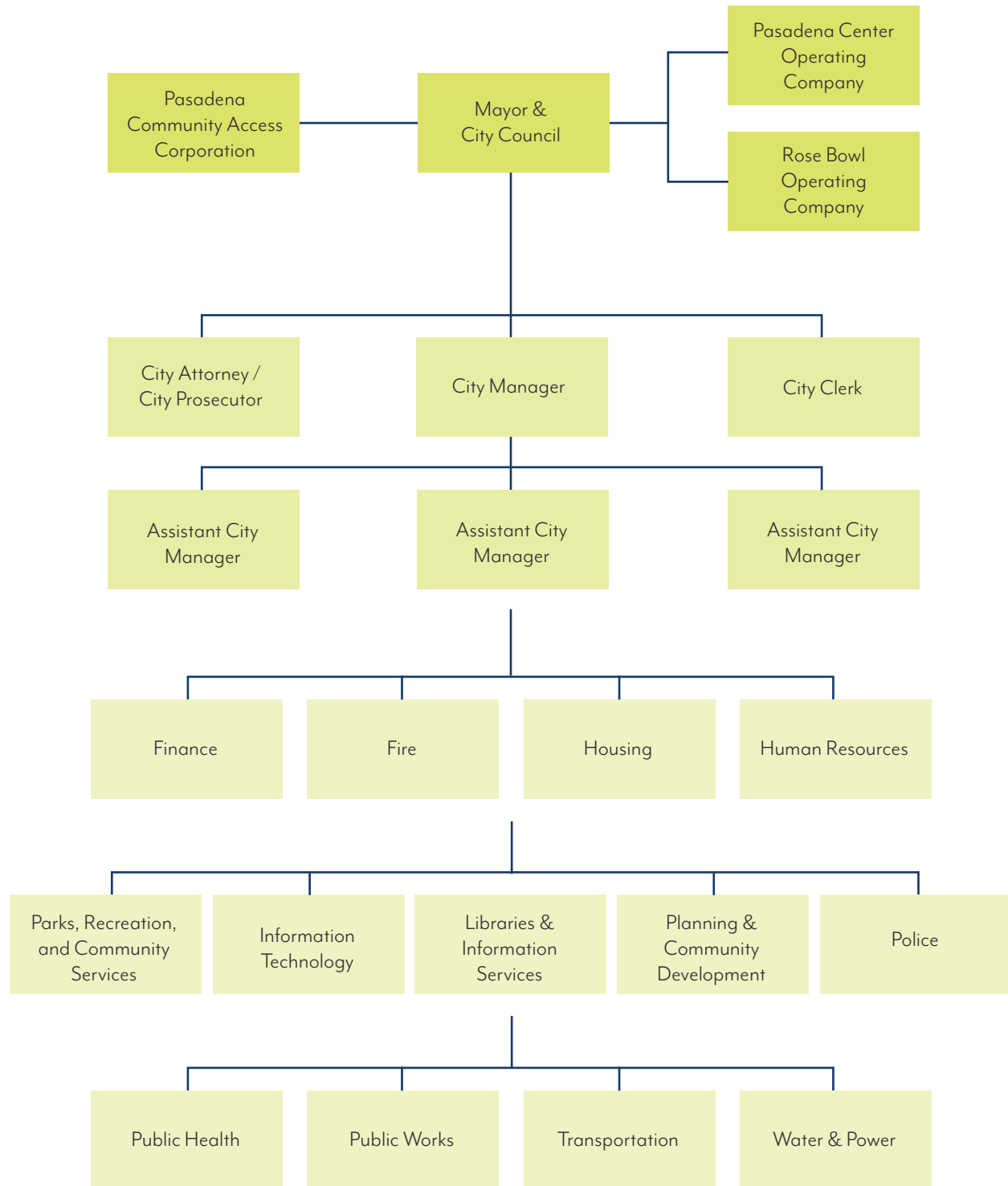


WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/planning/planning-division/community-planning/master-plans/>

Figure 7.2-1: Pasadena City Organization Chart



7.3 Funding

This section summarizes a variety of potential funding sources and financing measures that may help the City and community to implement the actions outlined in Table 7.1-1 and support the Specific Plan vision, goals, and policies. While some implementation actions can be implemented incrementally, others occur with development projects, and some others will require capital funding from a variety of sources. It is helpful to have outside funding to expedite implementation of the NLSP improvements as City revenues and fees summarized in the previous section are limited.

Typical outside sources of funding for pedestrian enhancements, streetscape improvements, public art, and affordable housing applicable to the NLSP are summarized in Table 7.3-1 and described in further detail in the following section including:

1. Land-Secured financing
2. Development and private sources
3. Regional and state programs

This list of sources is not exhaustive but is intended to provide a starting point for developing a funding strategy for North Lake improvements and programs. The programs listed in this chapter are relevant as of the time of Plan adoption, and funding programs are subject to change. As noted in the following section, grant applications for projects in the NLSP may be more successful if “bundled” with projects in other parts of the city.

7.3.1 LAND-SECURED FINANCING

Land-secured financing tools in California include the formation of benefit assessment districts, business or property-based business improvement districts, community facilities districts (CFDs), and others described below. Assessment tools and CFDs leverage the value of new development to capture additional tax revenues to finance infrastructure. The assessments can either be used to pay for improvements over time as the funds are collected, or can be bonded to make larger, up-front investments. One advantage of land-secured financing tools is that they can be applied toward district-wide improvements and are designed to ensure that properties benefiting from improvements also contribute to those public investments.

BUSINESS IMPROVEMENT DISTRICT (BID)/ PROPERTY-BASED BUSINESS IMPROVEMENT DISTRICT (PBID)

A BID is formed through assessments on businesses within the district, and a PBID is formed through assessments of property owners alone. Both BIDs and PBIDs are public/private partnerships created to support the revitalization of commercial neighborhoods. Establishing a BID/PBID is voluntary and subject to a majority vote of the area businesses or property owners. BIDs are somewhat limited in their ability to leverage funding and therefore typically provide a narrow scope of services. These may include marketing (e.g., signage, advertising), programming (e.g.,

Table 7.3-1: Summary of Funding Sources and Financing Measures by Category

Funding Source Category	Funding Source	Improvement Category			
		Pedestrian Enhancements	Streetscape	Public Art	Affordable Housing
Land-Secured Financing	Business Improvement District (BID)/Property-Based Business Improvement District (PBID)	✓	✓	✓	
	Community Facilities Districts (CFD, Mello-Roos)	✓	✓		
Development Private Sources	Development Agreement (DA) and Disposition and Development Agreement (DDA)	✓	✓	✓	✓
	Foundation and Corporate Sponsorships	✓	✓	✓	✓

Table 7.3-1: Summary of Funding Sources and Financing Measures by Category (continued)

Funding Source Category	Funding Source	Improvement Category			
		Pedestrian Enhancements	Streetscape	Public Art	Affordable Housing
Regional, State, and Federal Programs	Affordable Housing and Sustainable Communities Program	✓	✓		✓
	Sustainable Communities Competitive Grants	✓	✓		
	Active Transportation Program (ATP)	✓	✓		
	Urban Greening Program	✓	✓		
	Environmental Enhancement and Mitigation (EEM) Program	✓	✓		
	California Infrastructure and Economic Development Bank (I-Bank)	✓	✓		✓
	Community Development Block Grant (CDBG) Program	✓	✓		✓
	Metropolitan Transportation Authority (Metro) Call for Projects	✓	✓		✓
	New Markets Tax Credit (NMTC)	✓	✓	✓	✓

street fairs), security (to supplement local police), and sanitation (to supplement local services). The four existing PBIDs in Pasadena are in the Central District Specific Plan area, and each is organized around an established commercial area: Old Pasadena, Pasadena Playhouse District, South Lake Property Business Improvement District, and the Pasadena Tourism Business Improvement District.

COMMUNITY FACILITIES DISTRICTS (CFD, MELLO-ROOS)

Mello-Roos financing is a discretionary financing mechanism that applies to real property owners within a Mello-Roos District, which is also known as a Community Facilities District (CFD). A CFD may be enacted by a two-thirds majority approval of residents living within the district boundaries or by two-thirds majority vote of landowners when there are fewer than 12 residents. A special tax, which is separate from property taxes, is imposed on real property

in an area that benefits from the public improvement. The amount of the tax is determined by the real property owners and is usually less than one percent of the home value at the time the home value was assessed for CFD funding. The newly formed district then seeks public financing through the sale of tax-exempt bonds that are serviced using the special taxes paid by homeowners over the course of the bond's term (typically 20 to 30 years). Through Mello-Roos, a project developer or property owner can access capital to build infrastructure and public improvements at below-market rates. The debt associated with those capital investments recourses back to the property owners rather than to the City.

7.3.2 DEVELOPMENT AND PRIVATE SOURCES

DEVELOPMENT AGREEMENT (DA) AND DISPOSITION AND DEVELOPMENT AGREEMENT (DDA)

A DA is a voluntary but binding contract between a property owner and the jurisdiction in which the property is located that lays out the rules and conditions for development. A DA can give the jurisdiction greater control over the development process to define the form and nature of the development and to specify provision of community benefits such as affordable housing or off-site infrastructure improvements. For the developer, a DA may provide a level of certainty about the land use requirement and assurance the project will be exempt from future changes in the regulatory code. Benefits to both public and private parties include greater latitude in approval methods for new and creative local land use and flexibility in meeting regulatory requirements. The complexity and time required to negotiate a DA makes it appropriate mainly for larger-scale multi-phase projects.

A DDA is also a voluntary binding contract between a developer and jurisdiction. Like a DA, it provides flexibility to tailor a project to meet both developer and jurisdictional needs. A DDA differs from a Development Agreement in that it also entails the sale or lease of City-owned land. A DDA is necessary for disposition of former redevelopment properties held by successor agencies.

FOUNDATION AND CORPORATE SPONSORSHIPS

Private funds may also be raised for a specific use that implements the vision for the Specific Plan. A variety of foundations provide funding for community-based planning, resilience and sustainability initiatives, art installations, and other programs, including the Kresge Foundation, the National Endowment for the Arts, Kaiser Permanente, Citi Foundation, and Bloomberg Philanthropies.

7.3.3 REGIONAL, STATE, AND FEDERAL PROGRAMS

AFFORDABLE HOUSING AND SUSTAINABLE COMMUNITIES PROGRAM (AHSC)

The AHSC Program funds land use, housing, transportation, and land preservation projects to support infill and compact development that reduce greenhouse gas emissions. Administered through the California Department of Housing and Community Development (HCD), funding for the AHSC Program is provided from the Greenhouse Gas Reduction Fund, an account established to receive Cap-and-Trade auction proceeds. AHSC funds are split between Affordable Housing Developments or Housing Related Infrastructure and Disadvantaged Communities. The Plan area may qualify for Affordable Housing Developments or Housing Related Infrastructure funding. A contiguous area with at least one transit station/stop must be established for eligibility, including a flexible transit service route.

SUSTAINABLE COMMUNITIES COMPETITIVE GRANTS

The Sustainable Communities Competitive Grants fund transportation planning activities such as planning for active transportation, safe routes to schools, transit services, vision zero, complete streets, freight corridors, social equity, and integrated land use and transportation planning. Grants are available on an annual basis and through a competitive application process managed by Caltrans.

ACTIVE TRANSPORTATION PROGRAM (ATP)

The ATP funds projects that encourage increased use of active modes of transportation to increase the proportion of trips accomplished by biking and walking, increase safety and mobility for non-motorized users, advance the active transportation efforts of regional agencies to achieve Greenhouse Gas (GHG) reduction goals, enhance public health, ensure that disadvantaged communities fully share in the benefits of the program, and provide a broad spectrum of projects to benefit many types of active transportation users. ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SRTS), into a single program.

URBAN GREENING PROGRAM

The Urban Greening Program funds a variety of improvement projects, including urban heat island mitigation and energy conservation efforts, green streets and alleyways, non-motorized urban trails that provide safe routes for travel between residences, workplaces, commercial centers and schools, and others. The program is part of California Climate Investments, a statewide initiative that puts billions of Cap-and-Trade dollars to work reducing greenhouse gas emissions, strengthening the economy, and improving public health and the environment — particularly in disadvantaged communities.

If several improvement projects were bundled together, there is potential for the Urban Greening program to fund some NLSP area improvements if the project can demonstrate improved multi-modal safety or quality of life.

ENVIRONMENTAL ENHANCEMENT AND MITIGATION (EEM) PROGRAM

The EEM Program was established by the Legislature in 1989 to fund environmental enhancement and mitigation projects directly or indirectly related to transportation projects. EEM Program projects must fall within one of three categories: highway landscape and urban forestry; resource lands; or roadside recreation. Projects funded under this program must provide environmental enhancement and mitigation over and above that otherwise called for under the California Environmental Quality Act (CEQA).

CALIFORNIA INFRASTRUCTURE AND ECONOMIC DEVELOPMENT BANK (I-BANK)

The I-Bank finances public infrastructure and private development through issuing tax-exempt and taxable revenue bonds, providing financing to public agencies, providing credit enhancements, acquiring or leasing facilities, and leveraging State and federal funds.

The Infrastructure State Revolving Fund (ISRF) Program provides financing to public agencies and non-profit corporations for 18 categories of infrastructure and economic development projects. ISRF Program funding is available in amounts ranging from \$50,000 to \$25,000,000, with loan terms of up to 30 years.

CAP-AND-TRADE PROGRAMS

The State administers a growing number of grant and loan programs, collectively known as the California Climate Investments Program (CCIP), that provide funding for projects and programs that reduce greenhouse gases (GHGs) and provide health, mobility, economic, and other co-benefits to communities throughout the state. Cap-and-Trade provides funding in three primary areas:

1. Transportation and Sustainable Communities
2. Clean Energy and Energy Efficiency Funding
3. Natural Resources and Waste Diversion Funding

Under each of these funding areas are numerous programs that have funding available for projects and programs that would either be contained within the NLSP area or benefit the Specific Plan area and the City as a whole. Programs with high applicability to the NLSP are summarized in this section, including the AHSC and Urban Greening programs.

COMMUNITY DEVELOPMENT BLOCK GRANT (CDBG) PROGRAM

The CDBG Program funds revitalization of neighborhoods, expansion of affordable housing and economic opportunities, and/or improvements of community facilities and services, principally to benefit low- and moderate-income persons or neighborhoods. Also eligible are the building of public facilities and improvements, such as streets, sidewalks, sewers, water systems, community and senior citizen centers and recreational facilities.

Operated by the U.S. Department of Housing and Urban Development (HUD), the Community Development Block Grant (CDBG) Program is a federal program that provides grants for economic development, public facilities, and housing rehabilitation. CDBG funds must provide benefits to low- or moderate-income individuals, prevent or eliminate slums or blight, or may be used for other emergency community needs, such as those related to a natural disaster. CDBG funds can be used for development purposes within low- or moderate income census tracts, or, if the development or activity is located outside of a low- or moderate-income census tract, funds must provide benefits to low- or moderate-income households.

METROPOLITAN TRANSPORTATION AUTHORITY (METRO) CALL FOR PROJECTS

Every other year, the Metro Call for Projects funds projects across seven modal categories, including pedestrian improvements, which is applicable for the NLSP. Metro is responsible for allocating discretionary federal, state and local transportation funds to improve all modes of surface transportation. Metro also prepares the Los Angeles County Transportation Improvement Program (TIP). A key component of TIP is the Call for Projects program, a competitive process that distributes discretionary capital transportation funds to regionally significant projects. Local jurisdictions, transit operators, and other public agencies are eligible to submit applications proposing projects for funding.

NEW MARKETS TAX CREDIT (NMTC)

The New Markets Tax Credit (NMTC), a federal tax initiative, could be used to stimulate investment in new development within the Plan area. The NMTC offers tax credits to investors who finance development in low-income communities. These credits are intended to finance minor gaps in project funding and to increase the rate of return for investors. New Markets Tax Credits can fund up to 30 percent of eligible project costs. Projects must create new jobs in the service area and should provide community benefits that would not otherwise be possible solely through private financing. Although residential development is not eligible for the program, commercial space in a mixed-use building or stand-alone commercial projects could be financed in part by the NMTC.

NEW AND FUTURE RESOURCES

As funding opportunities are realized and new funding becomes available, the City can continue to identify, monitor, and apply for other governmental funding sources that meet the City's and respective agencies' objectives over time. For example, recently established funding sources include:

- » Cap-and-Trade Transformative Climate Communities Local Partnership Program provides funding to counties, cities, districts, and regional transportation agencies in which voters have approved fees or taxes dedicated solely to transportation improvements or that have imposed fees, including uniform developer fees, dedicated solely to transportation improvements.
- » HCD Infill Infrastructure Program provides grant funding for infrastructure improvements for new infill housing in residential and/or mixed-use projects.

7.4 Infrastructure

The City's existing infrastructure systems and facilities are owned and operated by different departments and other public agencies such as the City's Departments of Public Works and Transportation, Pasadena Water and Power, the Los Angeles County Sanitation Districts and Metropolitan Water District of Southern California. These City departments and other public agencies have processes in place to evaluate existing resources, service area needs, and plan for system upgrades to support growth throughout the City, including the Plan area. The following section identifies how infrastructure facilities for transportation and traffic, wastewater, water supply, solid waste, storm water, and electricity will be provided to meet the anticipated growth.

The 2015 General Plan Update Environmental Impact Report anticipated residential and commercial growth for the entire City through 2035, including the eight Specific Plan areas. Specific information on the analysis and environmental determinations associated with the buildout of the General Plan within the Specific Plan area can be found in the NLSP Addendum to the General Plan.

7.4.1 TRANSPORTATION

The City has a well-developed transportation network of streets, sidewalks, bicycle facilities, and transit services. Three freeways provide regional access to and through the City: the Foothill Freeway (I-210), the Ventura Freeway (SR 134) and the Arroyo Seco Parkway (SR 110). The public transportation system that serves the City includes local bus services, regional bus routes, and light rail. Transit Services are provided by Pasadena Transit, Los Angeles Metropolitan Transportation Authority (LA Metro), the Los Angeles Department of Transportation (LADOT) Commuter Express, Foothill Transit and numerous other local transit providers. The City also has Class II bikeways, Class III bike routes, and enhanced bike routes. Additionally, the City has a connected network of pedestrian facilities, designated pedestrian-friendly zones, and upgraded traffic signal technology.

Pasadena DOT helps to implement the Mobility Element of the General Plan through the Bicycle Transportation Action Plan (2015), DOT's Complete Streets Program, Neighborhood Traffic Management Program, Safety Improvement Projects, and other programs and projects to enhance the safety and mobility of all modes of transportation. Land use as defined in the General Plan is included in the City's travel demand model which is used to determine the potential impact of new projects and the City monitors traffic operations to identify areas of concerns and address safety and mobility needs.

7.4.2 WASTEWATER SYSTEM

The wastewater system in the Specific Plan area is owned and operated by the City of Pasadena's Department of Public Works and Pasadena Water and Power (PWP), which consists of approximately 328 miles of gravity pipelines and conveys an annual average flow of approximately 14 million gallons per day (MGD).¹ Wastewater from individual services flows into the City's collection system. The City's wastewater collection system conveys untreated wastewater to the Los Angeles County Sanitation District's (LACSD) trunk sewer system for treatment via 92 separate connections.

The Water System and Resources Plan (WSRP) is PWP's 25-year strategy, updated every five years, which includes planning related to the treatment of wastewater, primarily residential. LACSD is responsible for the treatment of wastewater, primarily commercial, from the City. LACSD prepares an annual report that speaks to their mission, core values and major projects for the year. The 2019 annual report notes LACSD is working to turn waste into electricity, compost and other recycling commodities. LACSD works closely with cities to support them in compliance with state and federal regulations for solid waste, green energy, and wastewater.

The City updates the Sewer System Management Plan (SSMP) annually to identify a list of Capital Improvement Program (CIP) projects that take into consideration the age of facilities, construction materials, current use, capacity, and its condition. The City has undertaken several major projects to ensure sustained reliability of the sanitary collection system. Projects include sewer system improvements and capacity upgrades as well as modernization of pump stations, such as the Busch Garden and Rosemont Sewer Pump Stations.

Developments in the Specific Plan area are subject to wastewater-related requirements and standard conditions of approval, such as payment of development fees and implementation of site-specific Storm Water Pollution Prevention Plan for construction. Development projects are required to comply with all applicable solid waste regulations, including the California Integrated Waste Management Act and the City's Zoning Code Section 17.40.120 (Refuse Storage Facilities).

¹ City of Pasadena Sewer System Management Plan (2018) <https://www.cityofpasadena.net/wp-content/uploads/sites/29/Sewer-System-Management-Plan-SSMP-Final-Report.pdf>

7.4.3 WATER SYSTEM

PWP, a community-owned utility and a not-for-profit public service owned and operated by the City, serves as the water service provider in the Specific Plan area. The PWP water system includes 14 reservoirs with total storage capacity of 110 million gallons, 17 active wells, 19 booster stations, and 1 treatment plant (Monk Hill Water Treatment Plant).² PWP obtains a portion of its water from the local Raymond Basin and purchases imported water from the Metropolitan Water District of Southern California (MWD).

PWP is responsible for evaluating the current and projected needs of customers for potable and non-potable water in the City. The WSRP provides screening of alternatives to meet future demands with necessary infrastructure within operational and financial constraints. PWP's WSRP includes considerations for water quality, greater dependency on local water, groundwater basin stability, reliability of the distribution system, affordability, climate change uncertainties, and legislative and regulatory requirements as well as the treatment of wastewater, primarily residential.

In addition, every five years the City updates its Urban Water Management Plan (UWMP) as required by the California State water code, which includes an analysis of long-term water supply and demand planning for PWP. The 2021 UWMP update included the population projections and land use changes based on the most recent General Plan Update and identified that supplies will exceed demands under all hydrologic scenarios with implementation of additional supplies, such as recycled water and potable reuse, as well as with conservation measures.

7.4.4 SOLID WASTE SYSTEM

The Department of Public Works (DPW) collects solid waste from residences in Pasadena and competes with private haulers for commercial collection. Refuse hauling companies providing commercial solid waste collection are listed on the Department of Public Works Franchise List. Solid waste is disposed of at the following facilities: Calabasas Sanitary Landfill, Scholl Canyon Landfill, Puente Hills Material Recovery Facility, Southeast Resource Recovery Facility, Commerce Refuse-to-Energy Facility, Olinda Alpha Sanitary Landfill, and Frank Bowerman Landfill. All landfills are required to comply with numerous landfill regulations from federal, state, and local regulatory agencies and are subject to regular inspections from CalRecycle and the local enforcement agency, the California Regional Water Quality Control Board, and the South Coast Air Quality Management District.

DPW Operations Section oversees waste management in the City. The DPW is responsible for the solid waste collection

² City of Pasadena – PWP 'Where our Water Comes From' Webpage <https://pwp.cityofpasadena.net/water/>

and disposal for all residential properties within the City and private haulers compete for commercial collection services in the City in conformance with the City's Municipal Code Chapter 8.61. The Zero Waste Pasadena 2040 Plan (Zero Waste Plan) is DPW's 25-year strategic plan, to be reviewed and updated every three years, that seeks to reduce waste at the source and maximize diversion from landfills with the overall goal of striving for zero waste in the year 2040. The Zero Waste Plan identifies diversion potential, greenhouse gas reduction potential, and materials management.

Developments within the Plan area would continue to be accommodated by existing solid waste service providers and facilities. Future development projects would be subject to the California Green Building Code and solid waste reduction strategies under General Plan policies that continue to encourage the reduction of solid waste through sustainable building practices. Additionally, the City seeks to reduce its solid waste and landfill greenhouse gas emissions in accordance with the Climate Action Plan (CAP) that establishes a goal of reaching an 87% diversion rate by 2035. CAP implementation actions include the Zero Waste Plan, reporting annually on zero waste progress and optimizing waste diversion.

7.4.5 STORMWATER SYSTEM

The City provides storm drainage collection in the Specific Plan area and is responsible for operation and maintenance of the collection system. The system includes open channels, closed conduits, catch basins, laterals, manholes, and other associated facilities. The City has approximately 34 miles of storm drain pipes, over 13,000 basins and hundreds of culverts.

The City provides for the repair and replacement of the City's storm drain system and improvements to the storm drain facilities throughout the City on an ongoing basis. However, the City is proposing as part of the 2021-2025 CIP to develop a Storm Drain Master Plan (SDMP) that would include a comprehensive analysis for stormwater capture infrastructure, drainage areas, soil characteristics, and wellhead protection zones. Presently, the City relies on a complaint-driven process for storm drain repairs instead of a systematic program of preventative maintenance. The SDMP would serve as a planning guide for locating and sizing stormwater and drainage facilities. Adoption of a SDMP will assist in the self-reliance on the City's water supply and the Los Angeles National Pollution Discharge Elimination System (NPDES) compliance.

Developments within the project area would be required to adhere to applicable local, state, and federal regulations and standards, as well as implement site design measures, low-impact development, and best management practices (BMPs), including infiltration features that contribute to groundwater recharge and minimize stormwater runoff,

erosion, siltation, and/or flooding. The City is one of the permittees under the NPDES municipal storm water permit which means that any new development in the Plan area is subject to the Los Angeles Standard Urban Storm Water Mitigation Plan (SUSMP). The SUSMP addresses post-construction storm water pollution from new development projects.

7.4.6 ELECTRIC SYSTEM

PWP provides electric services in the Specific Plan area with an energy system consisting of 16,58 linear miles of overhead and underground power line, 11,163 poles, and 11 substations.³ The City owns and operates the Glenarm Power Plant that includes two power generating facilities. The system meets the City's power demand with 10 percent coming from PWP-owned generating facilities and the rest purchased from varied sources, both conventional and renewable, or through the wholesale energy market.⁴ Electrical infrastructure in the Plan area is located above ground on utility poles as well as below ground.

The Power Integrated Resources Plan (PIRP) is the PWP's guiding document for achieving internal power supply goals while upholding local, state, and federal mandates. The state requires that the PIRP be updated on a regular basis in conformance with the California Energy Commission regulations. The PIRP speaks to the City's commitment to shift the energy supply portfolio to low-carbon and renewable resources as well as exceeding state mandates for Renewable Portfolio Standard increase and greenhouse gas emissions reduction targets.⁵ The City also has an adopted Climate Action Plan that continues efforts to promote energy efficiency and reduce the City's dependency on traditional energy sources.

New developments in the Plan area would be required to comply with the California Energy Code, Part 6 of the California Building Standards Code (Title 24), CALGreen standards, the City's CAP, and the City's Green Building Standards Code, which collectively would increase efficiency and decrease consumption levels. Any new developments in the Plan area would require lateral connections to mainlines in coordination with existing utility service providers.

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4 City of Pasadena – PWP 'Where Our Power Comes From' Webpage <https://www5.cityofpasadena.net/water-and-power/power/>

5 City of Pasadena – Power Integrated Resource Plan (2023) <https://pwp.cityofpasadena.net/wp-content/uploads/2024/09/2023-Integrated-Resource-Plan-Pasadena-Water-and-Power.pdf>

7.5 Administration

7.5.1 GENERAL

The Specific Plan serves as the implementation tool for the General Plan and establishes the zoning regulations for the Specific Plan area. All development proposals within the Specific Plan area are subject to the procedures established herein, in addition to those procedures identified in Zoning Code Chapter 17.60.

The regulations and design guidelines in this Specific Plan subject to the Zoning Code and other City regulations will not become effective until that amendment process (by ordinance) is complete. Wherever the provisions and development standards contained in the Specific Plan conflict with those contained in the Zoning Code, the provisions of the Specific Plan shall take precedence. Where the Specific Plan is silent, the Director of Planning and Community Development or Zoning Administrator will interpret.

7.5.2 AUTHORITY

The City of Pasadena initiated and prepared the Central District Specific Plan pursuant to the provisions of California Government Code, Title 7, Division 1, Chapter 3, Article 8 (Sections 65450 through 65457). The law allows the preparation of specific plans as required for the implementation of the General Plan. Specific plans act as a bridge between the general plan and individual development proposals. They combine development standards and guidelines, capital improvement programs, and financing methods into a single document that is tailored to meet the needs of a specific area. Jurisdictions may adopt specific plans by resolution or ordinance.

The Specific Plan is the regulatory document guiding land use and development within the boundaries of the Specific Plan area. Upon adoption by ordinance, this Specific Plan will serve as zoning for the properties involved. It establishes the necessary plans, development standards, regulations, infrastructure requirements, design guidelines, and implementation programs on which subsequent project-related development activities are to be based. It is intended that local public works projects, design review plans, detailed site plans, grading and building permits, or any other action requiring ministerial or discretionary approval applicable to this area be consistent with this Specific Plan.

7.5.3 APPLICABILITY

All development proposals within the Specific Plan area are subject to those procedures identified in Article 1 and Article 6 of the Zoning Code.

7.5.4 INTERPRETATION, CONFLICT AND SEVERABILITY

A. Interpretation

In case of uncertainty or ambiguity to the meaning or intent of any provision of this Specific Plan, the Director of Planning & Community Development and/or the Zoning Administrator has the authority to interpret the intent of the provision in a manner consistent with the goals, policies, purposes, and intent established in this Specific Plan. Refer to Zoning Code Chapter 17.12 of the Zoning Code.

The Director may, at their discretion, refer interpretations to the Planning Commission for consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. All interpretations made by the Director may be appealed to the Planning Commission in accordance with the appeal procedures in the Municipal Code.

B. Conflict

In the event of a conflict between the provisions of the Specific Plan and the provisions identified in the Municipal Code, the Specific Plan shall prevail. For any other topical issue, development standard or design guideline, and/or regulation not addressed or otherwise specified in the Specific Plan, regulation and approval shall be carried out in accordance with the provisions of the Municipal Code, particularly Zoning Code Chapters 17.12 and 17.60. The particular section of code shall be based on the most appropriate or closely matching land use type or procedure, as determined by the Zoning Administrator.

C. Severability

If any section, subsection, sentence, clause, phrase, or portion of this Specific Plan, or any future amendments or additions, is for any reason held to be invalid or unconstitutional by the decision of any court or competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Specific Plan, or any future amendments or additions.

7.5.5 REVIEW AND APPROVAL PROCESS

All projects proposed within the Specific Plan area shall substantially conform with the provisions of this Specific Plan. Article 6 of the Pasadena Zoning Code sets forth development review requirements and processes for approval of projects.

Appendices

A.1	Definitions	146
A.2	Design Guidance for Tree Selection	148
A.3	Subcommittee Recommendations.	168

A.1 Definitions

Amenity zone: the portion of the sidewalk located above and adjacent to the curb, providing space for amenities such as parkways, outdoor dining, seating, trees, lighting, bicycle racks, bus stops, etc.

Building frontage: The horizontal distance, measured at grade, of building wall facing the street.

Building frontage zone: The portion of the sidewalk immediately adjacent to the building façade, providing space for planters, outdoor dining, sidewalk signage, etc. This zone may not be present on every street or block.

Curb zone: See 'amenity zone'

Façade: Any exterior wall plane of a building, ground level to top of roof.

Floor area ratio: Numerical value obtained by dividing the above-ground area of a building or buildings located on a lot by the total area of the lot.

Footprint: The total ground floor area of the combined structures on a site or project area defined by the perimeter of the building(s), including parking structures but excluding parking lots and non-occupancy structures.

Frontage zone: See 'building frontage zone'

Gross floor area (GFA): The total enclosed area of all floors of a building measured to the inside face of the exterior walls including halls, stairways, elevator shafts at each floor level, service and mechanical equipment rooms and basement or attic areas having a height of more than seven feet, but excluding area used exclusively for parking or loading of vehicles or bicycles.

Ground floor: The first habitable floor of a building closest to sidewalk elevation.

Mixed-use project: The combination of commercial and residential uses in the same structure, where the residential component is located either above (vertical mixed-use) or behind (horizontal mixed-use) the non-residential component. Non-residential uses are typically commercial uses.

Open space: For any form of open space (Common, Publicly Accessible, Private, etc), see Section 6.3.

Parkway: landscaped or permeable areas located within the amenity zone of the sidewalk.

Paseo: A publicly accessible open space that functions as a pedestrian passageway connecting a public street to another public street, alley, or internal public space. Subject to minimum dimension and design requirements established by the Specific Plan.

Plaza: A publicly accessible open space with access from a public street. Subject to minimum dimension and design requirements established by the Specific Plan.

Primary curb line: the face of the predominant curb of an individual block forming the edge of the street.

Project: Refer to PMC 17.80.020

Residential common space: Those portions of a residential use building not dedicated to residential units that provide common services for residents. This may include spaces such as, but is not limited to, lobby or common building entry, leasing center, gyms/exercise space, shared kitchen, recreation center, screening or living room, business center, mail room, or library. These spaces/ portions of the building may be permitted on the ground floor where residential units are not permitted subject to Specific Plan standards.

Setback: The horizontal distance by which a structure, parking area, or development feature is required to be separated from the property line or the sidewalk line where applicable. In some cases superseded by Setback range.

Setback range: Minimum and maximum horizontal distances by which a structure or development feature is required to be separated from the sidewalk line. This measurement is similar to a "build-to" line.

Sidewalk line: The line parallel the property line accommodating the required sidewalk width, measured from the curb face. Where a sidewalk width is not specified, the sidewalk line is the property line.

Sidewalk zones: The three portions of a sidewalk that together comprise the public realm between a building and the street. Sidewalk zones are defined by the Pasadena Street Design Guide and regulated by the Specific Plan.

Shared property line: The property line separating adjacent parcels.

Stepback: The horizontal distance by which an upper story structure or development feature is required to be separated from the property line or the sidewalk line where applicable. Regulated above a specified vertical distance.

Street frontage: The horizontal distance along the street, measured at grade, between property lines (or sidewalk line where applicable) that are perpendicular to the adjacent street.

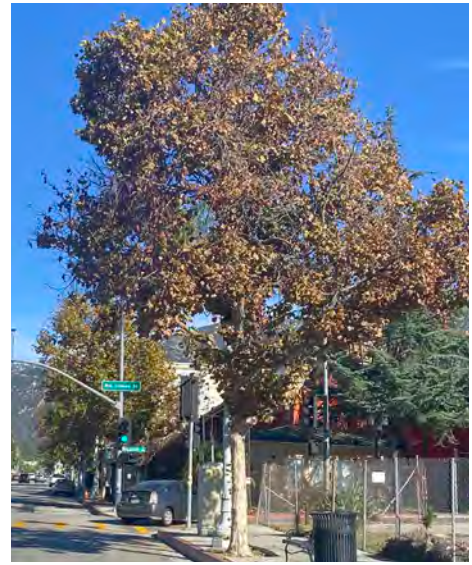
Subterranean: The level of a building, inclusive of parking or habitable space, located primarily below the ground level with a top plate of two feet or less above sidewalk elevation.

Transparent openings: Building openings (windows or doors) or transparent glazing that provide visual access into the structure.

Unbundled parking: Parking spaces, in any permitted configuration, rented or sold separately from the lease or purchase price.

Walk zone: The portion of the sidewalk dedicated to pedestrian movement, clear of any obstructions.

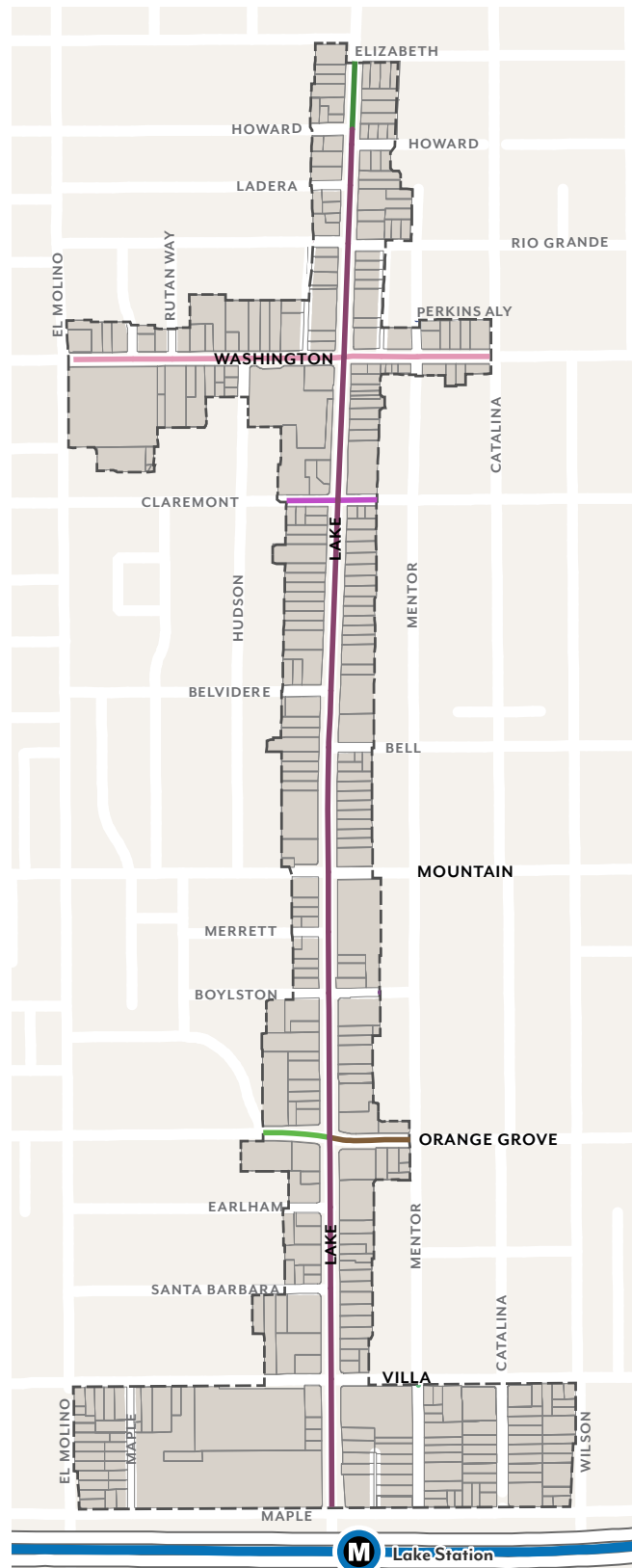
A.2 Design Guidance for Tree Selection



A variety of tree species can be found throughout the NLSP area both in street medians and in parkways.

While the City of Pasadena Department of Public Works' Master Street Tree Plan (MSTP) ultimately determines what tree species is planted in public right-of-way, this appendix to the North Lake Specific Plan (NLSP) is intended to guide discussions between the City and community when updating the MSTP for the Area. During the Specific Plan update process, opportunities were identified to better align North Lake Avenue's street trees with the vision, goals, and policies in the NLSP related to shade, climate resilience, stormwater capture, and supporting a vibrant public realm. This appendix includes a description of the existing street trees and recommendations for potential new species North Lake Avenue and key cross streets in the NLSP area that have the greatest need to enhance canopy, and which are not managed through an adopted streetscape plan already. While the guidance in this appendix focuses primarily on street trees located in parkways and tree wells, this appendix also provides guidance on opportunities to incorporate trees and landscaping in street medians and mid-block curb extensions.

Map A.2-1: Master Street Tree Plan Designations



Pasadena Master Street Tree Plan

- Species 1: London Plane
Species 2: Live Oak
- Species 1: Chinese Tallow Tree
Species 2: Live Oak
Species 3: London Plane
- Queensland Pittosporum
- Species 1: Live Oak
Species 2: Cork Oak
- Species 1: Live Oak
Species 2: Mesa Oak
Species 3: Southern Live Oak
- Species 1: Live Oak
Species 2: Mesa Oak
Species 3: Cork Oak
- [- - -] Specific Plan Area
- Metro A Line

A.2.1 GENERAL DESCRIPTION OF EXISTING DESIGNATED STREET TREE SPECIES

California Fan Palm (*Washingtonia filifera*)

The California Fan Palm is an evergreen tree characterized by its long narrow leaves and can be found natively in desert oases and stream courses in the southeastern region of California. The California Fan Palm can grow to be 80 feet in height with a tree canopy of 10-20 feet.

Camphor Tree (*Cinnamomum camphora*)

The Camphor Tree is an evergreen that is known for its fissured bark, stout branches, and pale green glossy leaves. The tree produces camphor oil when distilled from wood chips. These rounded trees can grow up to 70 feet with a canopy width of 50-60 feet. Camphor Trees are known to lift sidewalks and curbs due to their strong branches and trunks.

Chinese Tallow Tree (*Triadica sebifera*)

The Chinese Tallow Tree is a deciduous tree characterized by its rounded tree canopy and oval shaped leaves. The tree produces spectacular fall colors. Native to China and Japan, the Chinese Tallow Tree thrives in warm climates with adequate moisture. Once established, these trees can grow up to 40 feet in height with a tree canopy width of 30 feet. When given ample amounts of moisture, the trees are taller and more upright. Once established, the Chinese Tallow Tree can provide an expansive shade canopy for a portion of the year.

Coast Live Oak (*Quercus Agrifolia*)

The Coast Live Oak is an evergreen tree that is known for its large size often characterized by its rounded tree shape. The Coast Live Oak is native to Central and Southern California and is protected as a native tree species under Pasadena's local tree ordinance. The Coast Live Oak can grow up to 70 feet in height with a tree canopy width of 70 feet. The Coast Live Oak tree can provide a large spreading canopy which is often greater than the total height.

Cork Oak (*Quercus suber*)

The Cork Oak tree is an evergreen tree characterized by its tree height and rounded canopy. The underside of the leaf is light gray. The Cork Oak tree is also known by its grey and fissured bark which can be utilized to produce wine bottle corks. Native to the Western Mediterranean region, the Cork Oak can tolerate warm climates. Once established, these trees can grow up to 70 feet in height with a tree canopy width of 70 feet. The Cork Oak is an ideal tree for providing year-round shade.

Crape Myrtle (*Lagerstroemia indica*)

The Crape Myrtle tree is a deciduous tree native to China and Korea with showy magenta, pink or white flowers and dark green foliage that changes in fall to yellows, oranges, and reds. The species' remarkably smooth bark adds an additional visual interest to the tree. Once established, these can trees grow well in warm conditions with limited supplemental water, and are suitable for hot, sunny climates. The Crape Myrtle is a pedestrian-scaled tree that can grow up to 25 feet in height with a tree canopy width of up to 25 feet.

Deodar Cedar (*Cedrus deodara*)

The Deodar Cedar tree is an evergreen tree characterized by its conical treeshape, needle shaped leaves and dark gray bark. These trees are native to Eastern Afghanistan and Northern Pakistan. The Deodar Cedar can grow to be 60 feet in height with a canopy of 20-30 feet. Although these trees are drought tolerant, ongoing, or prolonged drought can cause the trees to die.

Holly Oak (*Quercus ilex*)

The Holly Oak tree is an evergreen tree characterized by its prominent umbrella form. Native to the Western Mediterranean Region it features spear shaped leaves. Once established, these trees can grow up to 60 feet in height with a tree canopy width of 60 feet. The Holly Oak's rounded shape allows for the tree to be an attractive street tree that provides year-round shade.

Horsetail Tree – River She Oak (*Casuarina cunninghamiana*)

The Horsetail Tree also known as the River She Oak is an evergreen tree is characterized by its use as a windscreen tree. These trees are native to Australia. The Horsetail Tree has the potential to reach a maximum height of 70 feet with a 30-foot canopy. These trees have a conical or rounded tree shape with bluish green, silver, or gray green leaves. The tree is often grown in California and can be found in urban environments due to its tolerance to smog and salt.

Incense Cedar (*Calocedrus decurrens*)

The Incense Cedar tree is an evergreen tree characterized by its conical shaped tree which can grow to be 90 feet tall, although their growth rate is initially slow. The Incense Cedar is native to the western region of North America. The Incense Cedar has a canopy width of 10-15 feet with green, scale-like leaves and red brown furrowed bark.

Italian Cypress (*Cupressus sempervirens*)

The Italian Cypress is an evergreen tree characterized by its narrow and upright form. This columnar-shaped tree can grow up to 70 feet with a canopy width of 10-20 feet. Known for its dark green leaves with a scale-like shape, the tree also produces fragrant flowers. The tree is native to Southern Europe and Western Asia and has shown to be tolerant to areas with smog.

Jacaranda (*Jacaranda mimosifolia*)

The Jacaranda is a partly deciduous tree renowned for its rounded tree shape and vibrant flowers. The Jacaranda is native to south-central South America and characterized by fern-like foliage and attractive violet-color flowers which bloom in spring and summer. Jacarandas can grow up to 50 feet in height with a tree canopy width of 30 feet. The Jacaranda tree is fast growing and drought tolerant and can provide shade for a majority of the year.

London Plane (*Platanus x acerifolia*)

The London Plane tree is deciduous tree that is characterized by its palmate leaves, tall, oval, or pyramidal tree shapes, and beautiful mottled white/gray bark. The London Plane tree is a hybrid of the American Sycamore and Oriental Planetree. Once established, the tree can grow up to 85 feet high and a tree canopy width of 70 feet. The London Plane is fast growing and is known for tolerating air pollution.

Maidenhair Tree a.k.a. “Ginkgo” (*Ginkgo biloba*)

The Maidenhair Tree, also known as the Ginkgo tree, is a deciduous tree that grows in an upright and conical manner. The Maidenhair Tree is native to China and is characterized by its fan shaped leaves. The leaves produce a brilliant gold yellow color in the fall. Once established, the tree can grow up to 100 feet in certain conditions but can commonly grow to 65 feet. The tree canopy width can extend up to 25 feet. When used as a street tree, the Maidenhair Tree can create a beautiful visual throughout the seasons. These trees grow well in urban environments – with resistance to air pollution and heat – and can thrive in confined conditions.

Mesa Oak (*Quercus Engelmannii*)

The Mesa Oak (also known as the Engelmann Oak or Pasadena Oak) is generally evergreen, but sometimes drought-deciduous tree that is known for its wide tree crown. The Mesa Oak is native to the Southern California and Baja California regions. The Mesa Oak has long elliptical leaves with a dull green/ blue-green sheen. Once established, the tree can grow up to 65 feet in height with a tree canopy width of 80-120 feet. Compared to the Coast Live Oak, the Mesa Oak typically grows in a more upright or columnar character in urban settings. When given a sufficient planting area, the Mesa Oak can provide extensive shade for most of the year.

Mexican Fan Palm (*Washingtonia robusta*)

The Mexican Fan Palm is an evergreen palm tree that is native to northwest Mexico. The Mexican Fan Palm can grow up to 100 feet with a canopy width of 5-10 feet. The fronds are typically dark green, and the tree produces black pea shaped fruit.

New Zealand Christmas Tree (*Metrosideros excelsa*)

The New Zealand Christmas Tree is an evergreen tree that is characterized by bright red flowers, and ovate dark green leaves. These trees are native to New Zealand and are sometimes used as screens or hedges. The tree is rounded with a maximum height of 40 feet and a canopy width of 30-35 feet. They can thrive in an urban environment given their drought resistance and smog tolerance.

Oriental Arborvitae (*Platycladus orientalis*)

The Oriental Arborvitae is an evergreen tree that is characterized by its green, scaled-like leaves and egg-shaped fruit. This tree is often used as a hedge or screen. It is a rounded tree with a maximum height of 20 feet and a canopy width of 10-15 feet. It needs a substantial amount of water and the possibility for good drainage.

Queen Palm (*Syagrus romanzoffiana*)

The Queen Palm is native to Southern Brazil and Northern Argentina, the Queen Palm grows to 50 feet tall with a canopy of 20-30 feet. This palm has green leaves with white flowers and fruits a medium orange drupe.

Queensland Pittosporum (*Auranticarpa rhombifolia*)

The Queensland Pittosporum is an evergreen tree that grows in an upright and rounded manner. The Queensland Pittosporum is native to Australia and characterized by its diamond shaped leaves. These trees can grow up to height of 35 feet, and a tree canopy width of 20 feet and grow well in urban conditions.

Southern Live Oak (*Quercus Virginiana*)

The Southern Live Oak is a semi-evergreen tree that is known for its stunning large, rounded tree shape. The Southern Live Oak is native to the Southern region of the United States and features leaves with a white colored underside. Once established, the tree can adapt to most environmental conditions other than high elevations. Southern Live Oaks can grow up to 80 feet in height and a tree canopy width of 100 feet. With such a large tree canopy, the Southern Live Oak can provide shade for most of the year.

Victorian Box (*Pittosporum undulatum*)

The Victorian Box is an evergreen tree that is known for its uses as an ornamental tree. The Victorian Box can grow up to 40 feet in height with a canopy width of 30-40 feet, once established. The tree is characterized with its green, oblong leaves and sticky fruit. Native to Australia, these trees be used as screens or hedges and are somewhat drought resistant.

Western Sycamore (*Platanus racemosa*)

The Western Sycamore is a deciduous tree that can grow to 80 feet with a canopy width of 20-50 feet once established. The Western Sycamore is native to California and is characterized by its palmately-lobed shaped leaves and white bark that is mottled with patches of gray, that peels away in large plates. The tree sheds its leaves during the winter and produces seedball fruit during the spring. These trees are very tolerant to heat and wind as well as provide shade for a majority of the year.

Windmill Palm (*Trachycarpus fortunei*)

The Windmill Palm is an evergreen tree that grows upright in a palm shaped manner. Native to Central China, these trees are characterized by their fan-shaped leaves. Windmill Palms can grow up to 30 feet, and a tree canopy width of 10 feet. Once established, the Windmill Palm can serve as a decorative street tree in urban conditions but does not provide much shade for pedestrian comfort.

White Mulberry (*Morus alba*)

The White Mulberry is a deciduous tree that grows in a rounded manner. The White Mulberry is native to China and can thrive in some drought conditions. When established, the tree can grow with a maximum height of 50 feet and canopy width of 30-50 feet. The tree produces flowers in the spring and edible blackberry-like fruits in the summer.

A.2.2 STREET SEGMENT EXISTING CONDITIONS

The following street segments have been analyzed for Design Guidance for Tree Selection.

- » **North Lake Avenue between East Elizabeth Street and Maple Street**
- » **East Washington Boulevard between El Molino Avenue and North Catalina Avenue**
- » **Orange Grove Boulevard between Elmira Street and North Mentor Avenue**

NORTH LAKE AVENUE (ELIZABETH STREET TO MAPLE STREET)

Street Segment	MSTP Designation(s)	Existing Tree Species and Count	
North Lake Ave. (Elizabeth St. to Maple St.)			
Elizabeth St. to Howard St.	London Plane, Live Oak	N/A	
Howard St. to Claremont St.	Chinese Tallow Tree,Live Oak, London Plane	London Plane	35
		Crape Myrtle	8
		New Zealand Christmas Tree	5
		Western Sycamore	5
		Queensland Pittosporum	4
		Live Oak	2
		Windmill Palm	2
Claremont St. to Mountain St.	Chinese Tallow Tree, Live Oak, London Plane	Queensland Pittosporum	19
		London Plane	17
		Live Oak	13
		Western Sycamore	3
Mountain St. to Orange Grove Blvd.	Chinese Tallow Tree, Live Oak, London Plane	Live Oak	7
		London Plane	5
		Queensland Pittosporum	5
Orange Grove Blvd. to Maple St.	Chinese Tallow Tree, Live Oak, London Plane	London Plane	40
		Maidenhair Tree	10
		Queensland Pittosporum	5
		Western Sycamore	2

MSTP designations along North Lake Avenue between East Elizabeth Street and East Maple Street include Chinese Tallow Tree, Live Oak, and London Plane trees. The existing street trees do have some overlap with the MSTP designations, including the established London Plane and Live Oak trees. However, many trees along Lake Avenue are not designated in the MSTP, including Crape Myrtles, New Zealand Christmas Trees, Western Sycamores, Windmill Palms, and Queensland Pittosporums.

London Plane

97 London Plane trees (*Platanus x acerifolia*) are planted along North Lake Avenue between East Howard Street and East Maple Street. London Plane trees are designated in the MSTP, and they are the primary trees along this portion of the corridor. Overall, the London Plane trees in this segment are thriving with healthy canopies and space to grow with heights ranging from 5-75 feet and canopies ranging from 5-45 feet. Occasionally, canopy growth appears hindered by nearby buildings. The consistency of the London Plane trees along North Lake Avenue fosters both a uniform visual character and shade canopy. A span of healthy London Plane trees can be found between 1326 North Lake Avenue and 1300 North Lake Avenue.

Crape Myrtle

Eight Crape Myrtle trees (*Lagerstroemia indica*) are planted along North Lake Avenue between East Howard Street and East Claremont Street with heights ranging from 10-20 feet and canopies between 5-25 feet. Crape Myrtles are not designated within the MSTP. Many of the Crape Myrtle trees along this corridor are planted in street medians along North Lake Avenue between East Washington Boulevard and East Claremont Street. The established Crape Myrtles have ample space for their canopies to grow outward. Trees located in street medians such as these contribute to the overall greening of the corridor, provide visual interest, and can contribute to traffic calming along busy North Lake Avenue.

Various Live Oak Species

22 Live Oak trees (*Quercus Agrifolia*) are planted along North Lake Avenue between East Howard Street and East Orange Grove Boulevard. These trees are designated within the MSTP. Live Oak canopy widths vary throughout the corridor from small canopies of only 5 feet on less established trees to up to 50 feet for more mature trees. Generally, the established Live Oak trees have space for their canopies to expand outward.

Maidenhair Tree

10 Maidenhair Trees (*Ginkgo biloba*) are planted along North Lake Avenue between Orange Grove Boulevard and East Maple Street. These trees are all located in a street median between Villa Street and Maple Street at 400 North Lake Avenue. The Maidenhair Trees vary in height with a range of 25-50 feet. However, their canopies are all close to their full potential with a range of 20-30 feet. Those with less space for their root systems on the southern portion of the median appear to be smaller than those with more space for expansion on the northern side. The trees both green the corridor and provide visual interest with their bright gold leaves in the fall. These trees are not designated within the MSTP.



London Plane located at 1506 N Lake Ave.



Maidenhair trees located at 400 N Lake Ave.

New Zealand Christmas Tree

Five New Zealand Christmas Trees (*Metrosideros excelsa*) are planted along North Lake Avenue between East Howard Street and East Claremont Street. These trees are not designated within the MSTP. The New Zealand Christmas Trees are predominantly located in the same area, in front of the North Lake Square Shopping Center at 1301 North Lake Avenue. These trees appear to be healthy with canopies ranging from 10-25 feet. The trees have room for their canopies to expand further, although in a few cases trees planted in the setback of the shopping center appear as though they could prevent the New Zealand Christmas Trees' canopies from reaching their full potential. The New Zealand Christmas Trees provide visual interest along the corridor with bright red flowers.

Western Sycamore

Ten Western Sycamore trees (*Platanus racemosa*) are planted along North Lake Avenue between East Howard Street and East Maple Street. These trees are not designated within the MSTP. Western Sycamore trees in this area generally appear healthy, but their relatively small canopies of 5-25 feet indicate that some are still in earlier stages of growth, and more mature trees may be constrained to a moderate size due to surrounding buildings, utility poles, or other amenities. When their canopies are full during the summer and fall months, these trees provide much-needed pockets of shade for pedestrians.

Windmill Palm

Two Windmill Palm trees (*Trachycarpus fortunei*) are planted along North Lake Avenue between East Howard Street and East Claremont Street, both located in the street median. These trees are not designated within the MSTP. The Windmill Palm trees contribute to visual interest, greening, and traffic calming along this segment of the corridor.

Queensland Pittosporum

32 Queensland Pittosporum trees (*Auranticarpa rhombifolia*) can be found on North Lake Avenue between East Howard Street and East Maple Street, specifically between East Howard Street and East Rio Grande Street. These trees are not designated within the MSTP. The Queensland Pittosporum's conditions vary from small canopies of only 5 feet to more established canopies of 30 feet. In some cases, their canopy growth appears to be impeded by nearby street infrastructure.



New Zealand Christmas Tree located at 1329 N Lake Ave.



Western Sycamore located at 1506 N Lake Ave.

EAST WASHINGTON BOULEVARD (EL MOLINO AVENUE AND NORTH CATALINA AVENUE)

Street Segment	MSTP Designation(s)	Existing Tree Species and Count	
East Washington Blvd. (El Molino Ave. and North Catalina Ave.)			
El Molino Ave. to Lake Ave.	Queensland Pittosporum	Mexican Fan Palm	38
		Queensland Pittosporum	8
		Victorian Box	10
		California Fan Palm	4
Lake Ave. to Catalina Ave.	Queensland Pittosporum	Mexican Fan Palm	7
		Queensland Pittosporum	2
		Italian Cypress	1

Trees along East Washington Boulevard within the Plan Area generally do not align with the designations within the MSTP. The MSTP designates the Queensland Pittosporum as the primary tree species for the segment, although only 10 are planted. There are a variety of non-designated species planted including Mexican Fan Palm, Italian Cypress, and Victorian Box trees. The Mexican Fan Palm tree is the primary street tree planted along the corridor, and the existing tree canopy leaves segments of the street without shade.

Mexican Fan Palm

45 Mexican Fan Palm trees (*Washingtonia robusta*) are found along East Washington Boulevard. These trees are not designated within the MSTP. The majority of the 45 Mexican Fan Palm trees can be found in-between El Molino Avenue and North Hudson Avenue. Although they provide a visually symmetrical frame to the street, there is a noticeable lack of shade along the sidewalk. These blocks border Washington Park, which provides a nearby concentration of tree canopy, but does not extend to the sidewalk area.



Mexican Fan Palm trees located at 700 E Washington Blvd.

Queensland Pittosporum

Ten Queensland Pittosporum trees (*Auranticarpa rhombifolia*) are found along East Washington Boulevard. These trees are designated within the MSTP. Dispersed on either side of the intersection with North Lake Avenue, the Queensland Pittosporum trees along East Washington Boulevard are interspersed with palms and other trees with smaller canopies, which doesn't allow for the development of consistent shade along the street. Generally, the Queensland Pittosporum trees appear to have space for their canopies to grow, although there are a range of canopy sizes found along Washington Boulevard between 5-25 feet, indicating that street conditions may be contributing to smaller canopy sizes for some of the trees.

Victorian Box

Ten Victorian Box trees (*Pittosporum undulatum*) are found along East Washington Boulevard. These trees are not designated within the MSTP. These trees are found between Palms and Queensland Pittosporum trees. Some of the Victorian Box trees along Washington Boulevard appear to be fairly young and don't yet have mature, shade-providing canopies.

California Fan Palm

Four California Fan Palm trees (*Washingtonia filifera*) are found along East Washington Boulevard. These trees are not designated in the MSTP. Due to their small canopies of only 15 feet, these trees do not contribute substantially to overall pedestrian comfort along the street. The California Fan Palm trees are found between North Lake Avenue and North Hudson Avenue and are interspersed with Queensland Pittosporum and Victorian Box trees. This segment of Washington Boulevard lacks formal parkways allowing minimal space for tree roots to grow.

Italian Cypress

One Italian Cypress tree (*Cupressus sempervirens*) can be found along East Washington Boulevard, located at 989 East Washington Boulevard. The tree has space to grow and has developed a robust and rounded canopy, providing shade to the sidewalk below.



Queensland Pittosporum located at 811 E Washington Blvd.



Italian Cypress located at 989 E Washington Blvd.

ORANGE GROVE BOULEVARD (ELMIRA STREET AND NORTH MENTOR AVENUE)

Street Segment	MSTP Designation(s)	Existing Tree Species and Count	
Orange Grove Blvd. (Elmira St. to North Mentor Ave.)			
Elmira St. to North Mentor Ave.	Live Oak, Mesa Oak, Cork Oak, Southern Live Oak	Holly Oak	4
		Live Oak	4

The MSTP designation for East Orange Grove Boulevard indicates Live Oak, Mesa Oak, and Southern Live Oak as the designated trees for this area. Currently, there are Holly Oak and Live Oak trees, which partially align with the MSTP designations. The intersection of East Orange Grove Boulevard and North Lake Avenue lacks substantial tree coverage and shade.

Various Oak Species

Three Holly Oak (*Quercus ilex*) and three Live Oak (*Quercus Agrifolia*) trees can be found on the portions of East Orange Grove Boulevard included in the NLSP. Oak species are designated in the MSTP for these segments. The Oak trees appear to be thriving with canopies ranging from 20-40 feet and heights between 15-35 feet. Although there is a robust Oak canopy found in the blocks of East Orange Grove Boulevard east and west of North Lake Avenue, there are few trees at the intersection of North Lake Avenue and East Orange Grove Boulevard, which lacks shade coverage on its northern and southwest corners.



A Live Oak tree near the intersection of N Lake Ave. and Orange Grove Blvd.



A Holly Oak tree at the intersection of N Lake Ave. and Orange Grove Blvd.

A.2.3 GUIDANCE FOR FUTURE TREE SELECTION

Trees play an important role in the experience of a streetscape. Through physical character, type of shade, and seasonal variety in the form of flowers or changing foliage, trees have a significant influence on our perception of a streetscape corridor. In addition to functional placemaking selection criteria, tree species selection should follow urban forestry best practices and take into consideration resilience and future climate change impacts. The planting environment for street trees is harsh, with trees often being subjected to limited root zone volumes, minimal supplemental irrigation, pollution from car exhaust, pet waste, and high temperatures from urban heat island effect as well as light reflected from nearby glazing. As climate change continues, we can anticipate generally warmer temperatures and more extreme heat days; therefore, the role of shade trees in urban environments will become increasingly valuable for urban heat island mitigation and pedestrian cooling. To meet this need, trees species should be suitable to warmer conditions and extreme heat.

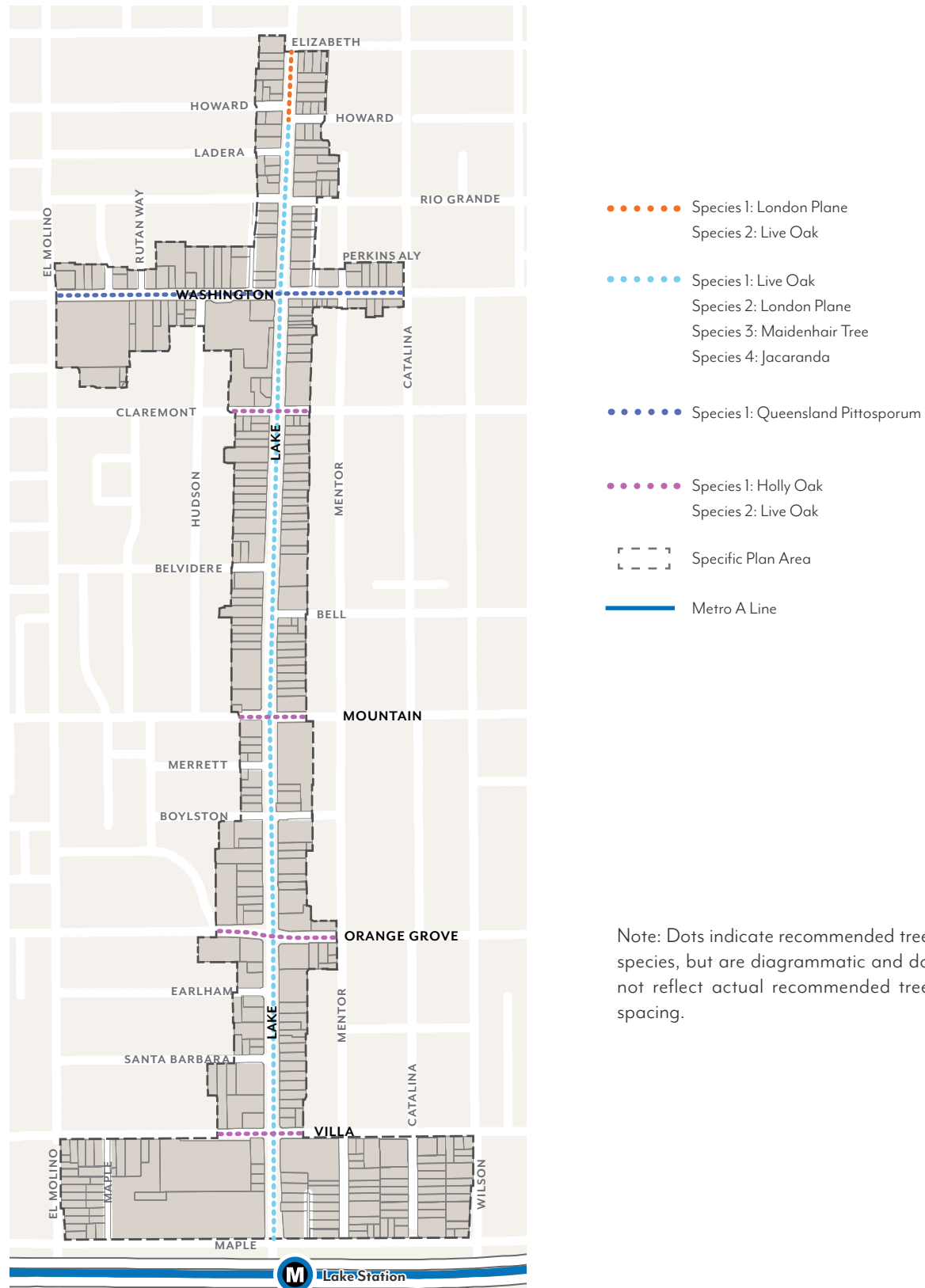


Established Crape Myrtle trees in the median in front of the North Lake Square Shopping Center at 1301 N Lake Ave.

Table A.2-1: Tree Species Recommendations by Street

Street Segment	Tree Species
» Lake Avenue (Elizabeth St. to Howard St.)	» London Plane (<i>Plantus x hispanica</i>) » Live Oak (<i>Quercus agrifolia/ Quercus virginiana</i>)
» Lake Avenue (Howard St. to Maple St.)	» London Plane (<i>Plantus x hispanica</i>) » Live Oak (<i>Quercus agrifolia/ Quercus virginiana</i>) » Maidenhair Tree (<i>Ginkgo biloba</i>) » Jacaranda (<i>Jacaranda mimosifolia</i>)
» Washington Boulevard (El Molino Ave. to Catalina Ave.)	» Queensland Pittosporum (<i>Auranticarpa rhombifolia</i>)
» Claremont Street (Hudson Ave. to Mentor Ave., partial blocks)	» Holly Oak (<i>Quercus ilex</i>) » Live Oak (<i>Quercus agrifolia/ Quercus virginiana</i>)
» Mountain Street (Wright St. to Mentor Ave., partial blocks)	» Holly Oak (<i>Quercus ilex</i>) » Live Oak (<i>Quercus agrifolia/ Quercus virginiana</i>)
» Orange Grove Boulevard (Elmira St. to Mentor Ave.)	» Holly Oak (<i>Quercus ilex</i>) » Live Oak (<i>Quercus agrifolia/ Quercus virginiana</i>)
» Villa Street (El Molino Ave. to Wilson Ave.)	» Holly Oak (<i>Quercus ilex</i>) » Live Oak (<i>Quercus agrifolia/ Quercus virginiana</i>)

Map A.2-2: Recommended Street Trees



NORTH LAKE AVENUE

(EAST ELIZABETH STREET TO EAST HOWARD STREET)

- » London Plane (*Plantus x hispanica*)
- » Live Oak (*Quercus Agrifolia/ Quercus Virginiana*)

North Lake Avenue currently has no street trees planted between East Elizabeth Street and East Howard Street, but there is an opportunity for future plantings to create a cohesive appearance and generate shade coverage along this portion of the street. Based on their existing success along the North Lake Avenue corridor, London Plane and Live Oak trees are recommended to remain designated for this street segment in the MSTP.



An example of a Live Oak tree

NORTH LAKE AVENUE

(EAST HOWARD STREET TO EAST MAPLE STREET)

- » London Plane (*Plantus x hispanica*)
- » Live Oak (*Quercus Agrifolia/ Quercus Virginiana*)
- » Maidenhair Tree (*Ginkgo biloba*)

The North Lake Avenue corridor from Howard Street to East Maple Street comprises a variety of deciduous and evergreen trees. The deciduous trees provide vibrant colors and flowers throughout the different seasons of the year. The evergreen trees in the corridor offer a consistent visual identity and contribute to the overall streetscape of the North Lake corridor. The London Plane is the primary existing street tree in the corridor. There are 102 trees currently planted along the corridor.

London Planes and Live Oak trees are recommended for future plantings along North Lake Avenue, and the trees should remain designated in the MSTP for North Lake Avenue due to their compatibility with existing plantings and anticipated development typologies. Maidenhair Trees and Jacarandas are recommended as seasonal accent trees to provide splashes of color to the streetscape experience, especially at significant intersections and landscaped street medians (see Map A.2-3). The Maidenhair Trees provide pops of fall colors from their leaves, while Jacaranda would provide visual interest in the spring and summer with their purple flowers, complementing the corridor's existing and future London Plane and Live Oak trees.



Maidenhair Trees in a street median between E Maple St. and E Villa St.

EAST WASHINGTON BOULEVARD
(EL MOLINO AVENUE TO NORTH CATALINA AVENUE)

- » Queensland Pittosporum (*Auranticarpa rhombifolia*)

The intersection of East Washington Boulevard and North Lake Avenue is currently lacking shade coverage, due to the lack of trees with substantial canopies and a predominance of Mexican Fan Palms. The existing Mexican Fan Palm trees do not provide shade or enhance pedestrian comfort, so the Queensland Pittosporum is recommended to remain as the designated tree for East Washington Boulevard. The Queensland Pittosporum can help provide seasonal shade for pedestrians and improve the pedestrian experience at the intersection.



An example of a Queensland Pittosporum

EAST CLAREMONT STREET

- » Holly Oak (*Quercus ilex*)
- » Live Oak (*Quercus Agrifolia*/ *Quercus Virginiana*)

The intersection of East Claremont Street and North Lake Avenue lacks shade coverage. However, Claremont Street has a healthy tree canopy along both sides of the street. Claremont Street has a variety of tree species planted, including California Fan Palm, Horsetail Tree, Live Oak, Oriental Arborvitae, Holly Oak, and White Mulberry. In keeping with MSTP designations and continuing to build shade canopy along the street, the recommended species for Claremont Street are Holly Oak and Live Oak trees. Oak trees currently planted in the subarea appear to be thriving. Planting Oak trees at this intersection can contribute to the establishment of a healthy tree canopy.



An example of a Holly Oak tree

EAST MOUNTAIN STREET

- » Holly Oak (*Quercus ilex*)
- » Live Oak (*Quercus Agrifolia/ Quercus Virginiana*)

The existing streetscape for the intersection of East Mountain Street and North Lake Avenue is comprised of a mix of Oak trees and Mexican Fan Palm trees. The Mexican Fan Palm trees do not provide sufficient shade for the public realm. The various Oak trees, when mature, will provide substantial shade coverage for the intersection. The setback buildings along the intersection could allow for additional trees, providing more shade. Oak species are included in the MSTP for this area and can thrive, eventually providing substantial shade coverage.

ORANGE GROVE BOULEVARD

- » Holly Oak (*Quercus ilex*)
- » Live Oak (*Quercus Agrifolia/ Quercus Virginiana*)

Various Oak species characterize the intersection of North Lake Avenue and East Orange Grove Boulevard, with four Holly Oak trees and four Live Oak trees planted at the intersection. It is recommended that various Oak species be planted in the future given the thriving examples of these trees throughout the NLSP area.



An example of established Holly Oak trees

EAST VILLA STREET

- » Holly Oak (*Quercus ilex*)
- » Live Oak (*Quercus Agrifolia*/ *Quercus Virginiana*)

The intersection of North Lake Avenue and East Villa Street contains various Oak tree species that provide ample shade. While the MSTP has designated the Mesa and Live Oak trees for East Villa Street, different Oak tree species are planted, as well as the Camphor Tree. The southeast side of East Villa Street has substantial shade coverage along the entirety of the block due to the current plantings of the Holly Oak tree. Tree recommendations correspond to the intersection of East Villa Street and North Lake Avenue, with room for additional shade coverage, especially on the western side of the intersection. Given the many thriving Oak species already planted along this corridor, continuing to plant Oak species along East Villa Street is recommended for consistency with the MSTP.

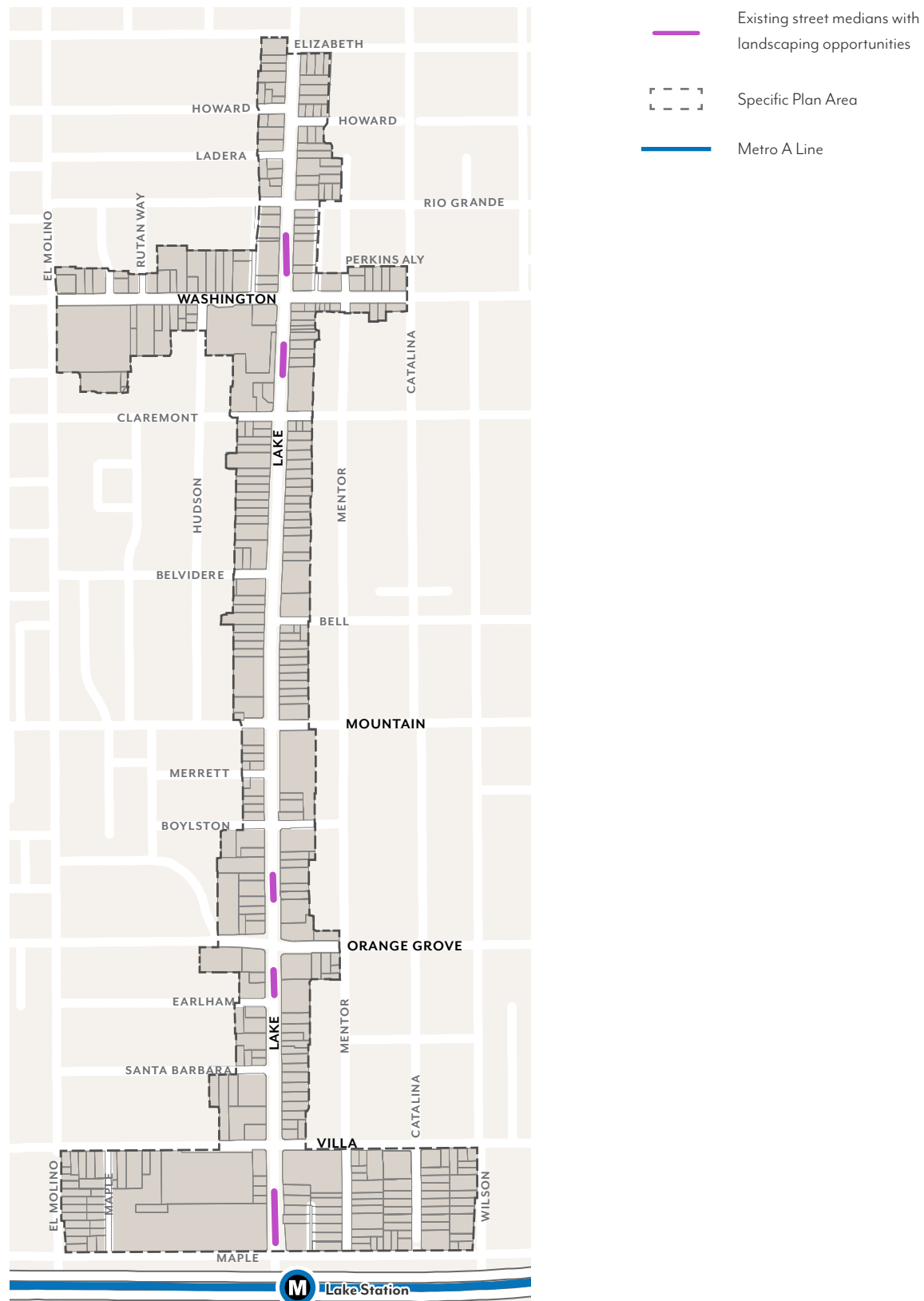


An example of an Oak species



An example of a Live Oak tree

Map A.2-3: Opportunities for
Landscaped Medians



A.3 Subcommittee Recommendations

DATE: September 12, 2024

TO: Community Members and City Staff

FROM: Planning Commission Subcommittee on the North Lake Specific Plan “Public Realm” Section
(Commissioners Cole, Delgado, Hunt Hernandez, Sims)

North Lake Specific Plan “Public Realm” Section Recommendations

INTRODUCTION

North Lake Avenue is the most prominent boulevard in the northern half of the City and a central artery for our community. Unlike South Lake Avenue, a successful retail and commercial street, North Lake Avenue functions as a *strove*, with the worst characteristics of both a street and a road. As a street, the current space and scale of North Lake Avenue are devoted to cars and trucks with a right-of-way disproportionately wide that does not accommodate well pedestrians and bicycles. As a road, it conveys a high volume of through traffic without a walkable, attractive public realm to support a healthy mix of uses for a successful street.

To ensure it lives up to its highest potential, reflecting the strength and vitality of the surrounding neighborhoods, North Lake Avenue needs a realistic community-based placemaking and placekeeping design and structure. The City of Pasadena, not the private sector, is responsible for creating and maintaining the Public Realm. Rather than being leftover space between buildings, the Public Realm is the communal expression of *place* that reinforces the vision and quality of life in the planning area. It cannot be left to chance. A vibrant Public Realm is the key to creating a valuable community as well as improving the value of private development.

Absent a sustained and bold commitment by the City to transformation and revitalization (akin to its successful effort in Old Pasadena), this North Lake Specific Plan Update will suffer the same failure as the earlier plans adopted in 1997 and 2007.

DEFINITIONS [insert into text here or at end in separate ‘Definitions’ Section]

To comply with and implement the City’s 2015 General Plan, the North Lake Specific Plan shall address North Lake Avenue as a *complete street* and *greenstreet*, as well as its *streetscape*.

Complete Street

Smart Growth America’s definition of a *complete street*—embodied in Pasadena’s Mobility Element--focuses on sustainable means of transportation to minimize environmental impacts while creating streets that are safe for everyone regardless of age, ability, or mode of transportation. Complete streets include physical improvements that benefit pedestrians, bicyclists, and mass-transit users while minimizing the focus on single-occupancy vehicles. The *complete street* concept also embraces the *greenstreet* principles of incorporating methods of mitigating stormwater runoff.

Greenstreet

Closely related to the definition of a *complete street*, the City of Seattle further describes a *greenstreet* as a public right-of-way that through design and operations prioritizes pedestrian and open space over

other transit modes. The purpose is to enhance and expand public open space while reinforcing desired land use and transportation patterns. It also focuses on the environmental aspect of mitigating stormwater runoff through creating vegetated stormwater treatment areas that can become a major theme in the design of the street.

Streetscape

The term *streetscape* has been used narrowly to mean the character of the space between building frontages and the curb (i.e., the sidewalk) or include related elements such as public open spaces, alleyways, and paseos. However, a larger and more useful definition means everything that makes up the scene on a street, as well as incorporated principles of *complete streets* and *greenstreets*. Typical elements include the roadway itself, flanking buildings, sidewalks, street trees, street lights, street furniture (permanent benches, trash receptacles, etc.) in addition to adjoining public or publicly-accessible open spaces. Collectively, these elements create an outdoor environment, a place for people to circulate, gather, and be outside.

RECOMMENDATIONS FOR TRANSFORMING THE PUBLIC REALM

The following recommended goals, objectives, and related policies are to be incorporated into the “Public Realm” section of the North Lake Specific Plan to transform the planning area into a special place consistent with the visions embodied in both the 2015 General Plan and the 2007 Amended North Lake Specific Plan.

VISION FOR NORTH LAKE AVENUE

The vision for North Lake Avenue is a result of a community planning effort. That effort is an instrument for both imagining and then implementing related design principles resulting in a street for people. According to Pasadena’s 2015 General Plan, the vision for the North Lake Specific Plan is

...to transform its automobile-oriented character with pockets of commercial and residential uses into a well-designed and attractive corridor supporting multiple travel modes including transit, bicycling, and walking with clusters of distinctive places for shopping, dining, and living. These will serve and be accessible to residents of adjoining neighborhoods....

[Pasadena 2015 General Plan Land Use Element, page 35]

Simply stated, the Vision for North Lake Avenue is to become a *Great Street*, the most important ingredient in creating a memorable urban place. As the heart of the North Lake Specific Plan Area, the Avenue will be a primary destination, a public arena for interchange and commerce, where the public life of the community occurs in a welcoming, inclusive environment. North Lake Avenue now lacks surrounding buildings of consistent scale and compatible design, vital to creating a vibrant ‘outdoor living room.’ As a *great street*, the Avenue will function for the community like an outdoor living room, a protected well-defined place for people to be outside, to meet, connect, linger, or conduct business. Appropriate building frontages will form the ‘interior walls’ of the outdoor room; the sidewalks, landscaping, roadways, and related public open spaces and greens its ‘floor’; its ‘ceiling’, the canopy of trees, overhangs, awnings, or umbrellas that modulate our experience of the weather and sky above. Experiencing North Lake Avenue will be protective while celebrating its history and mountain views.

GOALS

The Goals in implementing the Vision for North Lake Avenue as a *Great Street* are as follows:

1. Build a sense of place for the community and individuals by changing the current space and scale to create a vibrant outdoor living room.
2. Attract investment in business and mixed-use projects, especially housing and its retail and service needs.
3. Ensure visible care and upkeep critical to the safety, vitality, and sense of community pride and ownership in North Lake Avenue's urban street life. Above all else, the Avenue must be clean and safe.
4. Find effective ways to calm traffic movement along North Lake Avenue through symbols of enhanced walkability, traffic management in a two-way system, and design of traffic lanes to promote and protect pedestrian and bicycle use and safety.
5. Emphasize transit to and within the planning area rather than transit through it.
6. Manage and reduce congestion. For North Lake Avenue as a destination via all modes of transportation, understand that congestion is a byproduct of successful urban places. A place by definition that supports concentrations of social and economic activities within a pedestrian-scaled environment is going to be congested.
7. Address environmental impacts and the heat-island effect by reducing our carbon footprint. De-emphasize single-occupancy vehicle use and dependence on fossil fuels, reduce excessive impermeable paving, substantially increase the extent of the tree canopy, and ensure a memorable, walkable, delightful, and sustainable environment that is a model for other parts of the City.
8. Mitigate stormwater runoff through creative landscaping.
9. Establish a creative public art program that includes murals and reinforces a strong sense of place and continuity between the past, present, and future.

OBJECTIVES: How to Make North Lake Avenue a *Great Street*

1. Fund and Develop a Pedestrian Plan and Implementation Strategy

The Pedestrian Plan will provide the foundation for planning and developing the Public Realm for the North Lake Specific Plan Area. It will provide the blueprint for implementing pedestrian-oriented capital improvements to make it easier, safer, and more interesting to walk along the Avenue as well as to and from adjoining neighborhoods. The Plan will describe the existing conditions facing pedestrians on North Lake Avenue, analyze walking data and trends, describe best practices successfully used in other cities, and provide the basis and justification for funding and implementing capital improvements aimed at walking.

2. Design a Comprehensive Streetscape Plan for North Lake Avenue that incorporates principles of beautification, complete streets, and greenstreets.

Following adoption of the North Lake Specific Plan Update, the City shall develop and design a Comprehensive Streetscape Plan through a community outreach and participation effort, which is expected to include a citizen-lead steering committee. The Specific Plan's zoning will enable more residents to 'live above the store' along the Avenue and provide the needed 'eyes on the street' for safety and self-policing that will also support new community-serving businesses. The purpose of the Comprehensive Streetscape Plan—in concert with the zoning—is to transform North Lake Avenue physically into a *great street*. It serves as a catalyst for improving the visual character of the planning area while respecting, preserving, and celebrating its historic fabric and context. The Plan will establish a unique sense of place, providing pedestrian amenities, and supporting wellness through walking and biking. The 'new' North Lake Avenue will be clean and safe, no longer serving solely as an arid thoroughfare but as the heart of the surrounding neighborhoods. It will increase the customer base and property values, and generally enhance the quality of life for the planning area and the City as a whole in a sustainable way.

3. Fund installation and maintenance of the Comprehensive Streetscape Plan through public and private investment based on the City's binding commitment to direct adequate public resources to achieve the goal.

Along with the Pedestrian Plan studies, as first steps in the process, the City shall conduct an economic study of the North Lake Specific Plan Area while surveying and engaging the property and business owners. Prioritize City investment, apply for State and Federal grants, and support establishing a Property-based Business Improvement District (P-BID). Also, explore establishing an Enhanced Infrastructure Funding District (EIFD), a Parking District, or other financing mechanisms. Consider creating a set-aside fund with fees collected from new development projects in lieu of required onsite improvements to install the Comprehensive Streetscape Plan, which Pasadena has done in the past. Explore alternate funding sources such as fundraising and private sector donations for planting and maintaining street trees and creating a Landscaping Improvement District (LID) for adjacent neighborhoods. In the event that parking meters are installed along North Lake Avenue, explore the use of meter revenues in excess of expenses to fund improvements (as was done in Old Pasadena).

4. Designate a City staff lead to spearhead a planning and design team to coordinate and develop the Public Realm. This would include developing and installing capital improvements recommended by the Plan and an operation and maintenance program to ensure their viability over time.

Establish a City staff lead person for North Lake Avenue to work closely with residents, businesses, private investors, and relevant City departments—especially Planning, Transportation, and Public Works—to ensure all capital improvements are installed and maintained in a comprehensive, cost-efficient, and resource conscious manner. Ensure the Housing and Economic Development departments actively and consistently market investment opportunities along North Lake Avenue, especially housing opportunities and community-serving retail that supports residents. To be most effective, improvements to the Public Realm should be implemented within the same period of time to achieve the best possible effect (as was done in Old Pasadena).

POLICIES: Twenty Five Ways to Transform North Lake Avenue into a Great Street

1. Create an equitable balance of space usage between sidewalks and streets in the public right-of-way. The ideal allocation is 50/50 between pedestrians and vehicles. This translates on North Lake Avenue, with its 105-ft. right-of-way, to about 50 feet for pedestrian-oriented use and the remaining 50 feet for all other modes of travel and on-street parking.
2. Provide two vehicular travel lanes in each direction going north and south along North Lake Avenue. The travel lanes shall be narrowed as much as possible to deemphasize the width of the right-of-way yet still provide two safe travel lanes. Vehicle lanes shall not exceed 12 feet in width. The design speed along North Lake Avenue shall not exceed 35mph.
3. Eliminate the current shared vehicle/bicycle travel lanes. Instead, install one-way protected bicycle lanes on the east and west side of the Avenue or a two-way bikeway on one side protected from vehicle traffic by trees, landscaping, and parking lanes without the barrenness and confusion of the Union Street bikeway (see the City of Rosemead as a model). Include bicycle and pedestrian safety improvements around the Metro stop/freeway overpass zone that connect to North Lake Avenue.
4. Maximize off-street parking as much as possible. Eliminate on-street parking from Orange Grove to Mountain Streets where fronting drive-thrus. Where critically needed to serve businesses without onsite parking, reduce the number of on-street spaces or re-configure them to be more efficient (i.e. install diagonal parking, create parking inlets, etc.).
5. The center turning lanes shall be removed or minimized as much as possible for safe turning at intersections. Useable space freed up within the right-of-way shall be dedicated to pedestrian or community use. The additional space can be used for central, pedestrian-oriented medians, *ramblas* (such as in the City of Lancaster), and wider sidewalk areas.
6. The additional right-of-way gained from eliminating on-street parking and center turning lanes shall be used to establish or enlarge useable medians along all or part of North Lake Avenue to benefit the community. The additional space can be used for walkable, landscaped 'park blocks' like in Portland or West Hollywood, especially from Orange Grove to Washington flanked by drive-thrus. Or, create central diagonal-parking and event space like the Moule & Polyzoides-designed *ramblas* for Downtown Lancaster; or community gardens like in Vancouver and Oakland; or some combination.
7. "Pedestrian refuges" shall be installed at the end of each median or somewhere in the middle of longer ones where needed, to ensure that those who cannot cross the entirety of the street can wait safely in the median until they can continue crossing to the other side, like sections of South Lake Avenue.
8. Lighted crosswalks at selected locations in between the controlled intersections shall be installed, with flashing lights embedded in the road and on signs beside it to alert drivers.
9. Install diagonal crossings (like in Old Pasadena) and continental "ladder" crosswalks at intersections. These will serve to mark North Lake Avenue better as a pedestrian street, significantly lessen the mixing of pedestrians and vehicles, and reduce traffic speeds. At all signalized intersections ensure enough

time for pedestrians to cross the street comfortably and safely. Default to pedestrian crossing prior to initializing green lights for vehicles.

10. At all bus stops, ensure a permanent tree canopy or other shading devices to shelter transit users from the elements. Provide adequate lighting, comfortable seating, and real-time transit information.

11. The entire streetscape shall be landscaped using water-conserving low-impact development (LID) and smart irrigation techniques. Incorporate permeable paving, bio-swales, and other methods to control runoff and capture stormwater for onsite reuse.

12. Landscaping should include a diversity of species—native, non-native Mediterranean, non-invasive and other water efficient types—to increase urban resilience, create habitat for insects and birds, and serve as drought-tolerant, waterwise demonstration gardens.

13. Plant appropriate, evergreen street trees—as designated in an updated adopted Street Tree Plan for the North Lake area—that provide a year-round canopy with a spread preferably of 18 feet or more, planted at about 10 - 30 feet on center intervals, depending on the species. Consider planting citrus and other food-producing species for resilience and as accents to celebrate our climate and heritage.

14. Sidewalks shall be 18 feet in width as a baseline, with 10 feet apportioned for the path of travel next to the building property line, known as the “pedestrian zone,” and the remaining 8 feet reserved for trees, utility access, street furniture, known as the “curbside zone.” Variations in these two zones may be designed to accommodate outdoor dining, installing public art, street vending, and other activities.

15. Widened sidewalk areas shall be landscaped and installed at one time or phased along entire blocks or sub-districts in a coordinated way. Interest and continuity shall be provided in the design that also connects people safely to the median and across the street. Provide hardscape variation in shape, width, and quality paving materials to promote strolling and discovery and differentiate the sub-districts. Accommodate activities such as ‘people-watching,’ street vending, and street or farmers markets, with kiosks, newsstands, and outdoor dining, shaded from the California sun. Include permanent street furniture (benches and chairs for gathering and lingering, waste receptacles, water faucets, etc.), public art and murals, pedestrian-scaled lighting, bicycle facilities, and other amenities.

16. To give variety to the design of the corridor, special landscaping treatments shall be installed as decorative nodes or ‘plazas’ at four major intersections along North Lake Avenue: at Villa Street, Orange Grove Blvd., Mountain Street, and Washington Blvd.

17. The east side of Boylston Street shall be closed off or vehicle access curbed to stop illegal cut-through traffic from fast food businesses on North Lake Avenue into the neighborhood.

18. Identify and acquire potential paseos, parklets, and greenspaces. Reduce onsite parking requirements, support shared parking arrangements on private property, or develop district-wide parking or through some other means reduce excess space for cars, and instead acquire easements for passive greening (non-active recreational use) and public space throughout the planning area.

19. At the Food for Less property, the onsite parking requirements shall be reduced to eliminate the heat island effect from the massive, underutilized sea of asphalt. The City shall find the means to incentivize the property owners to redevelop the site with underground parking and added housing,

with residential units above stores or offices along the North Lake Avenue frontage, and community-serving retail to support residents. The same shall be undertaken for the CVS property, the former Big Lots property, and others with parking fronting North Lake Avenue.

20. As much as possible, underground parking shall be required for redeveloped sites. In all cases, new onsite surface parking lots abutting North Lake Avenue shall be prohibited.

21. The City shall develop and support the means to “Park Once and Walk” along North Lake Avenue by working with property and business owners to increase shared parking opportunities in publicly owned parking lots and to reduce on-site parking requirements to the extent possible. Include some public parking in the redevelopment of the former Kaiser site at Lake and Villa as a start.

22. New curb cuts shall be prohibited along North Lake Avenue. As uses change, current ones shall be relocated or eliminated to increase the safety and continuity of the pedestrian realm.

23. Emphasize allowing outdoor dining along the street frontage that is protected from vehicle emissions through screening or landscaping. Outdoor dining shall not be allowed adjacent to residential uses, especially in parking lots or open space in the rear of restaurants, without approval of a Minor Conditional Use Permit (MCUP).

24. Develop an incentivized façade improvement “Main Street”-type program to encourage and assist existing owners to upgrade their properties flanking the public right-of-way through building repair, remodeling, re-painting, and onsite landscaping. Partner with Habitat for Humanity, the American Institute of Architects, or other professional, service, educational, faith-based, and neighborhood organizations, as well as businesses to provide *pro bono* design assistance, labor, and materials.

25. Develop, adopt, maintain, and enforce a comprehensive, coordinated sign program for the North Lake Specific Plan Area. Pay special attention to reducing the amount of signage that obscures street-facing windows to increase transparency and a sense of safety for pedestrians. An over-street sign at the start of the streetscape or a system of banners or other artistic means shall be installed to demarcate the planning area, enhance its unique visual character, and create a sense of entry and arrival (look to Culver City and Santa Monica as examples).



A Potential View of North Lake Avenue; image generated by Megheti DerBoghossian

IMPLEMENTATION ACTIONS AND TIMELINE

<u>ACTION</u>	<u>DUE DATE FROM PLAN ADOPTION</u>
Designate City Staff Lead	Upon adoption
Create City Task Force w/Citizen Participation	Upon adoption
Reach Out to Property and Business Owners	Upon adoption/On-going
Conduct Economic/Demographic Study	One Year
(Concurrently) Develop Pedestrian Study and Plan	One Year
Develop Funding Sources	On-going
Include in Capital Improvements Program	One Year
Develop Property-Based Improvement District (PBID)	Three Years
Establish Partnerships with Non-Profits	On-going
Design Comprehensive Streetscape Plan	Three Years
(Concurrently) Revise Street Trees and Landscaping Plan	
(Concurrently) Establish Public Parking District	
Install Streetscape Plan Improvements	Five Years
Reconfigure Right-of-Way and Install Infrastructure	
Install Paving, Street Trees, Landscaping, Street Furniture	



Looking south from the intersection of Lake Ave. and Washington Blvd., circa 1950.

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